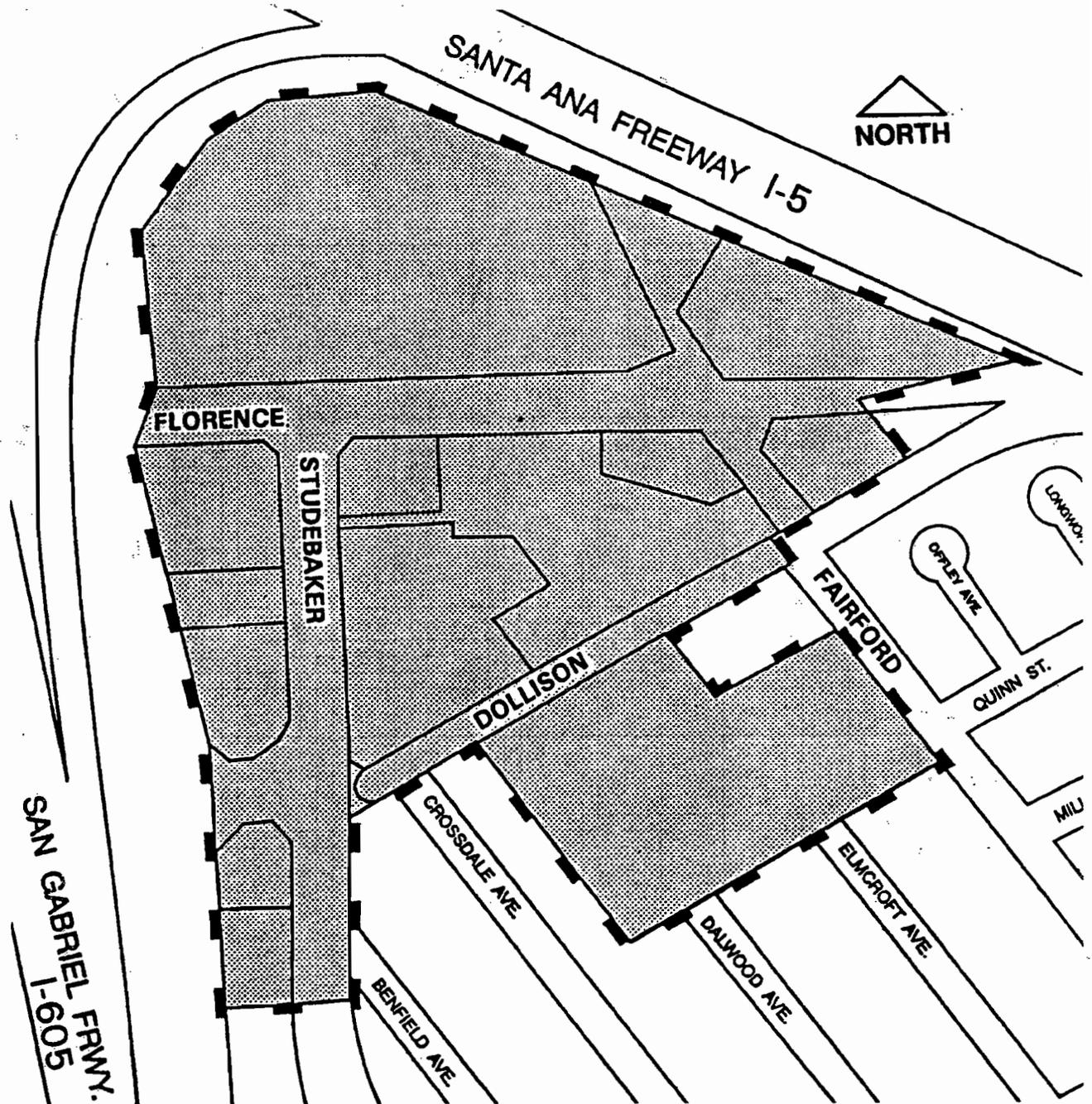


# CITY OF DOWNEY FLORENCE AVENUE/I-5 SPECIFIC PLAN 90-1



June 1991



# **CITY OF DOWNEY FLORENCE AVENUE/I-5 SPECIFIC PLAN 90-1**

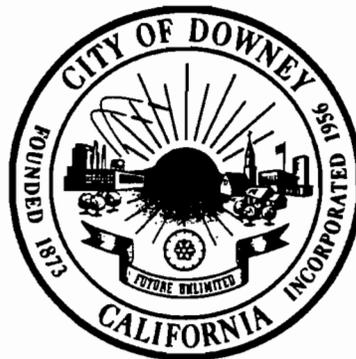
**JUNE 1991**

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# FLORENCE AVENUE/I-5 SPECIFIC PLAN 90-1

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**City Council Ordinance**

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RESOLUTION NO. 1437

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF DOWNEY  
RECOMMENDING THAT THE CITY COUNCIL ADOPT  
SPECIFIC PLAN 90-1

THE PLANNING COMMISSION OF THE CITY OF DOWNEY DOES RESOLVE AS FOLLOWS:

SECTION 1. The Planning Commission hereby finds, determines, and declares that:

- A. On March 27, 1990, the Community Development Commission of the City of Downey initiated preparation of Specific Plan 90-1 (SP 90-1) to guide high-intensity commercial development of approximately 39 acres at the interchange of the I-5 and I-605 freeways.
- B. A Draft Environmental Impact Report (DEIR) has been prepared in accordance with the California Environmental Quality Act (CEQA), including mitigation measures reducing and eliminating impacts identified in the DEIR as a result of developing the area with high-intensity, predominately auto-related uses. The Planning Commission has studied the environmental impacts of the project in accordance with the requirements of CEQA and the City of Downey Environmental Guidelines and recommended that the EIR be certified by the City Council.
- C. On April 3, 1991, the Planning Commission held a duly noticed public hearing on SP 90-1 and its EIR and have carefully considered all of the evidence and public comments received at said hearing.

SECTION 2. Having considered all of the oral and written evidence presented to it at said public hearing, the Planning Commission further finds, determines, and declares that:

- A. SP 90-1 is necessary and desirable for the development of the community and is consistent with the objectives of the general plan, as existing and proposed to be amended by General Plan Amendment 91-23; and
- B. SP 90-1 is consistent with the existing commercial land uses of the properties and furthers the public health, safety, and general welfare and will be compatible with and complementary to existing conditions and adjoining property; and
- C. SP 90-1 identifies specific uses that can be accommodated on the properties. Appropriate development standards have been established consistent with other commercial revitalization efforts in the community.
- D. SP 90-1 establishes circulation and highway goals and improvements for the type and quantity of traffic that is expected to be generated by the project. Utilities and services exist or are planned to adequately serve the project. The specific plan has examined and established services and infrastructure requirements to the satisfaction of the City.

- E. There are changed circumstances since the existing zoning ordinance became effective that warrant the specific plan. Based on the approximate 39-acre size of the property, specific standards are necessary to guide orderly and effective high-intensity commercial development of the area.

SECTION 3. Based on the findings set forth above, the Planning Commission hereby recommends to the City Council that the City Council adopt Specific Plan 90-1.

SECTION 4. Certified copies of this Resolution shall be transmitted to the Community Development Commission and to the City Council of the City of Downey.

PASSED, APPROVED, AND ADOPTED THIS 17th DAY OF APRIL, 1991.

---

Joyce Lawrence, Chairman  
City Planning Commission

I HEREBY CERTIFY that the foregoing Resolution was duly and regularly passed and adopted by the Planning Commission of the City of Downey at a regular meeting thereof held on the 17th day of April, 1991 by the following vote:

AYES: 5 Commissioners: Carter, Lumsdaine, Gaston, McCaughan, Lawrence

NOES: 0

ABSENT: 0

ABSTAINED: 0

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Theresa Sutton, Secretary  
City Planning Commission

# **SECTION I – INTRODUCTION**

## **A. Purpose, Goals and Objectives**

The Florence Avenue/I-5 Specific Plan serves as the planning and development regulations for future improvements to an area of approximately 39 acres adjacent to two major Southern California freeways. It is the intent and purpose of this document to provide a comprehensive set of land uses, development plans, development regulations, design guidelines, and implementation programs to ensure quality development consistent with the Land Use Plan and the goals, policies, and objectives of the Downey General Plan.

This Specific Plan provides regulations and standards that unify and coordinate future development, circulation, landscaping and utility improvements into a comprehensive development program. The Specific Plan text and graphics serve as the ongoing development code for the specific plan area, to achieve the following goals:

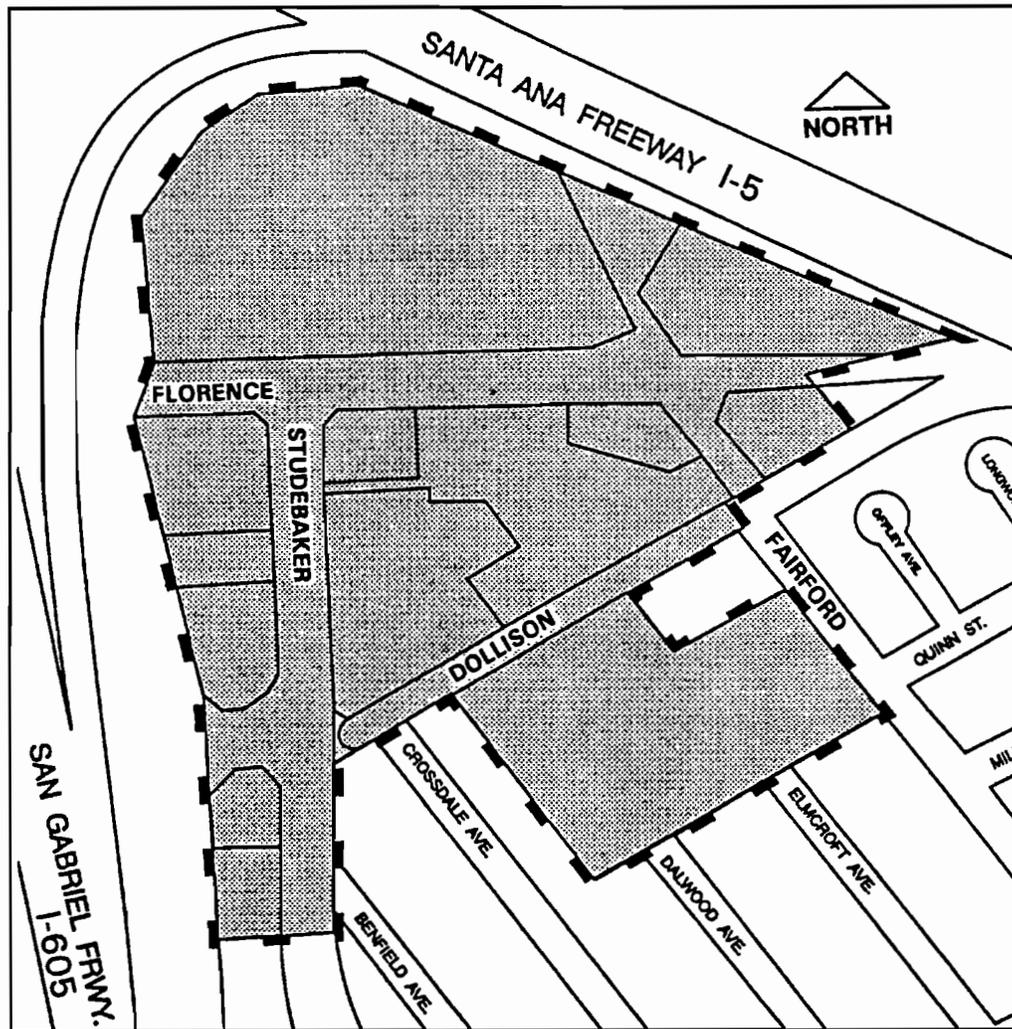
- Provide the specific plan area with a unified character through compatible development and complementary site planning and building design.
- Provide standards that will ensure attractive, high-intensity, revenue-producing commercial developments as existing obsolete developments within the study area are recycled.
- Prevent adverse impacts of commercial development on residential neighborhoods south of the specific plan area.
- Establish zoning tailored to the characteristics and needs of each site within the study area. Eliminate existing inconsistencies between zoning and land use.
- Phase-out approvals of zone exception permits.
- Improve vehicular and pedestrian circulation.
- Improve domestic water supply, fire flow, and drainage.

## **B. Background**

The future of the commercial areas near the Santa Ana and San Gabriel River Freeways has been a concern of the City of Downey for several years. In 1984, the sale of the former Fairford School site prompted several contentious public hearings as various types of development were proposed for the 6.1-acre parcel (Penske Honda eventually acquired the site and established its service/storage facility there under Conditional Use Permits). In 1986, when the former Gemco facility was vacated, the City Council established a building moratorium for the area to evaluate future uses. That moratorium expired in 1987. Concern for coordinating future development in the area persisted, however, and on March 27, 1990, the City Council established the boundaries of the Florence/I-5 Specific Plan Area (See Figure 1) and authorized preparation of this document. In August 1990, the district was designated as a proposed amendment area (#5B) to the City of Downey's Redevelopment Plan. A redevelopment plan separate from this document has

been completed. An environmental impact report has been prepared to evaluate the effects of both plans.

In 1990, City Staff met with property owners and major tenants within the study area to hear their ideas for the future of their properties and for the future of the Specific Plan area. The City hired Urban Design Studio (UDS) of Newport Beach to assist in site analysis and preparation of alternative development scenarios. After consultation with City Staff and site research UDS developed three alternatives. Alternative 1 promoted high-intensity commercial development with an emphasis on automotive sales and service. Alternative 2 promoted high-intensity commercial development emphasizing highway services such as hotels and restaurants. Alternative 3 proposed leaving the study area largely as it is, a mixture of commercial uses (with restaurants and similar uses replacing obsolete properties).



*Florence Avenue/I-5  
Specific Plan Area*

Urban Design Studio Developed an assessment matrix to assist the City in evaluating the three alternatives:

**LAND USE ALTERNATIVES ASSESSMENT MATRIX**

Alternative 1: High-Intensity Commercial—Automotive Emphasis

Alternative 2: High-Intensity General Commercial

Alternative 3: Medium-Intensity Commercial: Mixed Uses

<b>ASSESSMENT CRITERIA</b>	<b>ALT.1</b>	<b>ALT.2</b>	<b>ALT.3</b>
Desirability and compatibility of land uses	4	2½	3
Market feasibility: ease of implementation	4	1	3
Least impact to study area infrastructure	3	2	2
Adaptability to existing area land uses	2	2	4
Effective visual access	2	4	3
Profitability to landowners and City	4	4	2
Acceptance by local residents	2	3	3
Compatibility with current zoning	3	2	3
Least environmental impact	3	2	2
Adaptability to existing property lines	2	2	4
Least impact on existing circulation system	4	2	2

In addition, the City commissioned a market study to evaluate the feasibility of attracting a hotel to this area. That study concluded that the demand for additional hotels along this portion of the Santa Ana Freeway is small, given recent hotel construction in neighboring Norwalk and Commerce. Finally, several property owners expressed considerable support for the concept of creating a center oriented toward the sales and service of new vehicles. The City prepared Specific Plan 90-1 using this concept as the preferred alternative, although uses suggested in the other alternatives remain listed as permitted and conditional uses for various sites.

At a neighborhood workshop held in March 1991, residents of the surrounding area made suggested changes to the permitted land uses and to the circulation concept plan that have been incorporated into the plan.

**C. Zoning and General Plan Classification**

The regulations and standards presented in this specific plan will henceforth serve as the zoning for the specific plan area, as described in Section VIIB of this document. General Plan Amendment 91-23 will be processed concurrent with this specific plan to assure consistency between the General Plan and the zoning regulations presented.

## **D. Environmental Review and Alternatives**

The Environmental Impact Report (EIR) for the Florence Avenue/I-5 Specific Plan was prepared in compliance with the California Environmental Quality Act (CEQA). Pursuant to State CEQA guidelines and the City's Environmental Review Procedures, the City of Downey prepared an initial study. The City determined that the Project could result in environmental impacts and therefore required an EIR, which was prepared to guide the planning effort and to evaluate the consequences of implementing this specific plan. When certified, the EIR is applicable to all permits and approvals that implement the specific plan.

The EIR evaluates several land use alternatives, which are also mentioned elsewhere in this specific plan. The Preferred Land Use Alternative considers High Intensity Auto-Related Commercial Uses. Other alternatives considered are

- High-intensity, automotive and retail uses, including a large freestanding retail use, one restaurant, and auto service.
- High-Intensity, Highway Service uses such as a hotels, restaurants, and multi-family residential units.
- Medium-intensity, mixed use commercial development, including, offices, restaurants, and auto sales.

## **SECTION II – PROJECT DESCRIPTION**

### **A. Location/Area Description/History**

#### **1. Location**

The Florence/I-5 specific plan area is located in the easternmost portion of the City of Downey. The 39-acre study area occupies the apex of a triangle bounded by the I-605 freeway on the west, the City of Santa Fe Springs on the east and the City of Norwalk on the south. The City of Downey annexed the area in 1957, at a time when the land was agricultural. Today the specific plan area is commercial. The balance of the triangle is single-family residential.

#### **2. Area Description**

Physically, the study area is relatively flat with no distinguishing natural features. Several grade changes affect the study area, however. The San Gabriel River Freeway was constructed above grade. The land was built up to support the freeway and its approaches, resulting in a "wall" approximately 30 feet high that separates the specific plan area from the rest of Downey to the west. Similarly, land was built up for the Florence Avenue overpass of the Santa Ana Freeway. This causes the land to slope away from Florence Avenue along the north side of Site 4 and the south side of Site 5. These grade changes may influence how these sites are ultimately developed.

#### **3. History**

From the 19th century to the 1960s, the specific plan area was used for agriculture. Large parcels were arrayed along Florence Avenue, which jogged south at its present intersection with Little Lake Road and continued along what is now Dollison Drive. A smaller road called Easy Street (later Mayhew and Sanford Bridge Road) crossed the river from the northwest and passed through Site 1.

At the time of Downey's incorporation in 1956, the city's eastern boundary followed the San Gabriel River. At that time, northeastern Downey up to the river was already subdivided and developed with residences. Much of the land east of the river was still agricultural, and Los Angeles County considered the area in its planning to be within Norwalk's sphere of influence. The district joined Downey as Annexation #2 in October 1957 (Annexation #1 was the area between the Santa Ana Freeway and Telegraph Road). Although an integral and fiscally important part of the City since that time, the triangle area is served by separate water and school districts than the rest of Downey.

The San Gabriel River Freeway, completed in 1961, divided the area and forced the realignment of several streets, including Studebaker. The land to the west, adjacent to the river was developed as Wilderness Park in 1973. The above-grade freeway created an additional barrier between the specific plan area and the rest of Downey.

When the specific plan area became part of Downey in 1957, the agricultural land was zoned for single-family residences. Most of the land was developed commercially in the 1960s. The land west of Studebaker (Sites 8-12), was property left over from the freeway's construction that Caltrans sold and developed in the early 1970s. The Murata family—which owned much of the land in the triangle since 1931—continued to sell berries from a stand on Florence Avenue into the late 1980s and continues to occupy the 1890s farmhouse to the present.

In the 1960s, Site 1 north of Florence Avenue was developed as a Gemco membership retail store. Bob Spreen (now Penske) Cadillac was built on Studebaker Road, and became known throughout Southern California for its advertisements that touted its location "where the freeways meet in Downey." Site 6 was developed for nearly 30 years as Fairford Elementary School. With declining enrollment in the 1980s, the school site was sold and has become the service and new car preparation area for the Penske Cadillac and Honda Dealerships.

Most of the commercial development was permitted by zone exception approvals and occurred in a relatively haphazard pattern. At least four additional service stations once existed in the study area. The City's General Plans of 1963 and 1973 designated most of the area for commercial uses, and the 1977 zoning ordinance zoned most of the land C-2, but no master plan for the district was ever developed.

## **B. Opportunities and Constraints**

### **1. Opportunities**

The specific plan area presents several opportunities for high-intensity, high revenue-producing land uses including:

- a. Over 650 feet of frontage along the Santa Ana Freeway at grade.
- b. A developable parcel of nearly 10 acres north of Florence Avenue.
- c. Easy accessibility from two freeways.
- d. A prominent intersection of two busy streets.
- e. Constructed barriers on two sides that can buffer commercial land uses.
- f. Flat parcels

### **2. Constraints**

Several constraints also exist that present challenges to development of these properties. These include:

- a. Little sense of place. No identity—not even a name—for the specific plan area.
- b. Poor view of the specific plan area from freeways because of elevated highways, approaches, and overpasses, and because of mature freeway landscaping.
- c. Irregularly shaped and small parcels.
- d. Poor parcel access.
- e. Poor freeway identification. For example, exits from the I-605 are taken from ramps for other exits. Downey is noted on few freeway signs.
- f. Inadequate fire flow for many types of commercial development.
- g. Lack of unity in design.
- h. Congested intersection at Florence and Studebaker.
- i. Residential uses to the south.

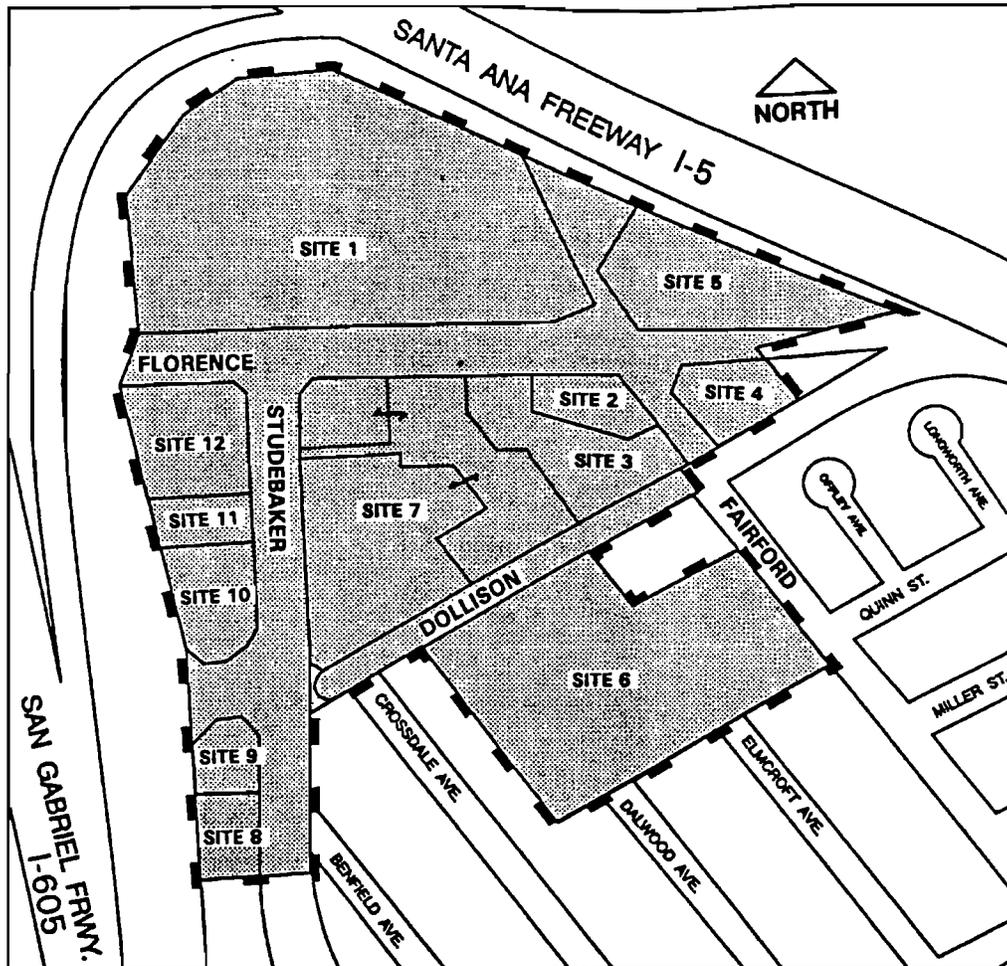
### **C. Land Use Plan/Development Concept**

The concept land use plan involves introduction and intensification of auto and auto-related uses—new car sales, service, and accessory retail—into the specific plan area. Over time, most of the existing uses that are not auto-oriented would be phased out.

The study area consists of 12 sites, most corresponding to existing property boundaries. Three commercial land use concepts were developed, each with a different emphasis: high-intensity auto sales, high-intensity general commercial, and medium-intensity commercial. Discussions with property owners and merchants and analysis of site and circulation constraints were used to compare these alternatives. Based on this analysis, the high-intensity auto-oriented alternative was chosen as the preferred concept for the area. The location of these parcels lends itself best to the following uses, which are considered the most efficient and the highest revenue-producing uses.

The following preferred uses were selected for each site, and constitute the land use plan developed in this document (see map on next page). Although these uses are preferred, other appropriate uses have been identified for each parcel and are listed with the site-specific development standards in Section III of this plan.

Site 1	Auto Sales	Site 7	Auto Sales
Site 2	Auto Sales	Site 8	General Commercial
Site 3	Auto Sales	Site 9	Auto Service Station
Site 4	Automotive Accessory Sales	Site 10	Auto Service Station
Site 5	Auto Sales	Site 11	Auto Sales
Site 6	Auto Service & Storage	Site 12	Auto Sales



**FIGURE 2**  
*For planning purposes, the specific plan area is divided into twelve sites.*

'Automotive Accessory Sales' includes retail sales of accessories such as bras, car covers, car stereo systems, cellular telephone systems, custom muffler pipes, custom wheels, driving lights, gauges, key fobs, mirrors, mud flaps, seat covers, spoilers, steering wheels, and wiper blades. It does not include sale and installation of body parts, major engine parts, mufflers, service station garage tools and equipment, or tires.

The land uses and development standards for the specific plan area are established by this specific plan. The permitted uses and development standards for all sites are a variation on C-2 zoning, except for that portion of Site 6 that is zoned OS (Open Space). That area is also governed by the land uses and development standards of this document, but those standards are more of a variation of OS zoning than C-2, so that residential uses will continue to be buffered from commercial activity.

The standards embodied in this specific plan could permit development of the study area as shown in the conceptual site plan on the following page. This concept does not represent a particular proposal, but provides an example of how structures and uses could be arranged according to the guidelines of this document.

## **D. Design and Streetscape Concept**

Unified and compatible building designs are goals of this specific plan. To achieve this, a set of design guidelines have been developed for all new construction. Section IV of this specific plan lists these standards in detail. This section summarizes the architectural concept.

New structures in the Florence/I-5 area will be constructed in an "Early California" style as defined by this specific plan. This style incorporates elements of Mediterranean, Spanish Colonial, and Mission Revival architecture.

Section IV also outlines design improvements for sidewalks, parkways, and street medians. These improvements include use of Mexican Fan Palms, in parkways and medians, and decorative brick pattern pavers at major intersections.

## **E. Circulation Concept**

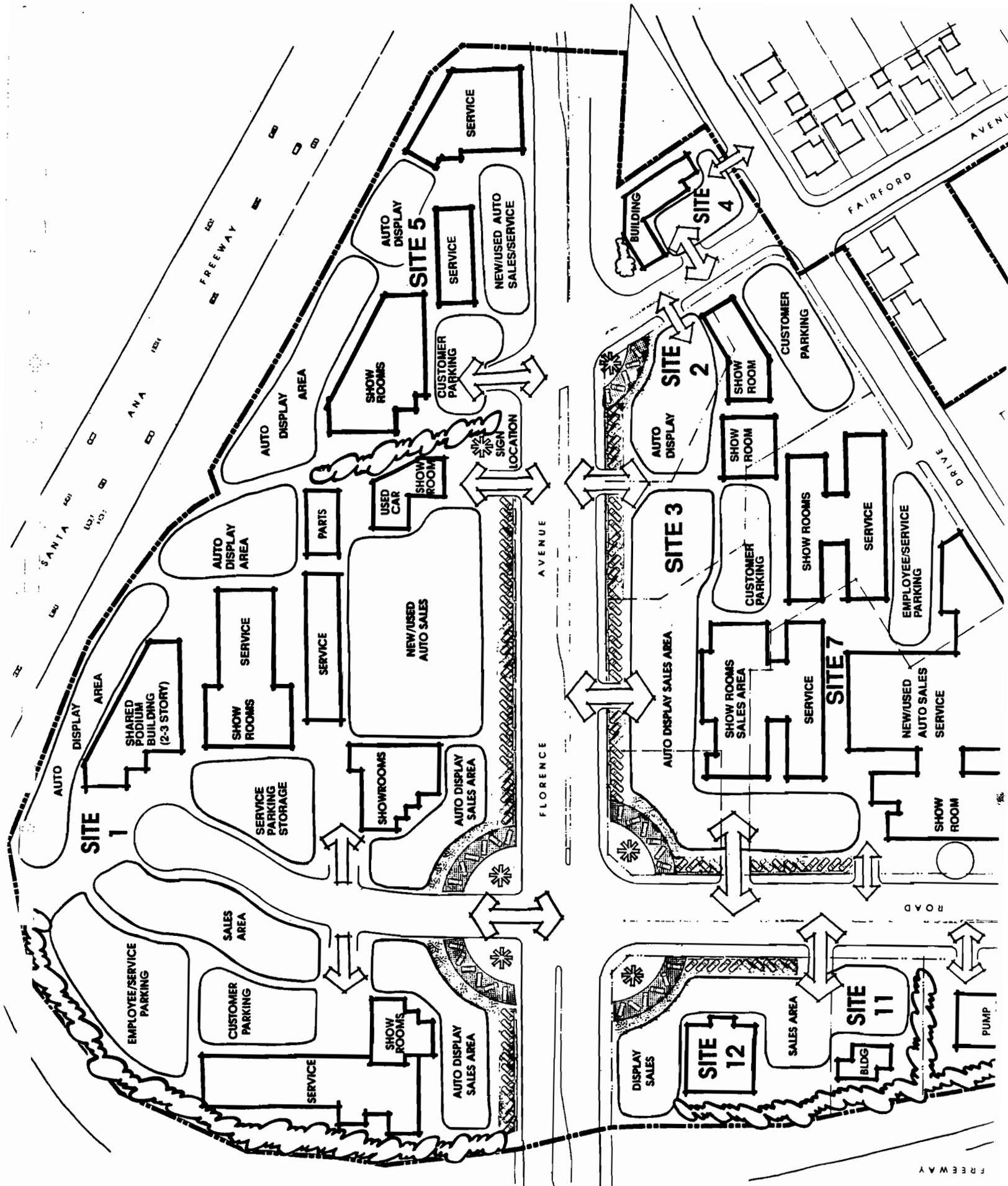
Development allowed by this land use plan may intensify traffic on the streets within the study area, but major circulation patterns will not change greatly. Florence Avenue and Studebaker Road will continue to be the primary arterials serving the properties involved. Improvements may occur at the major intersection of Florence and Studebaker to facilitate traffic movement and to identify the intersection as part of a planned district within the City of Downey. The development regulations of Section III are intended to ensure that traffic impacts to Fairford Avenue and Dollison drive are minimized.

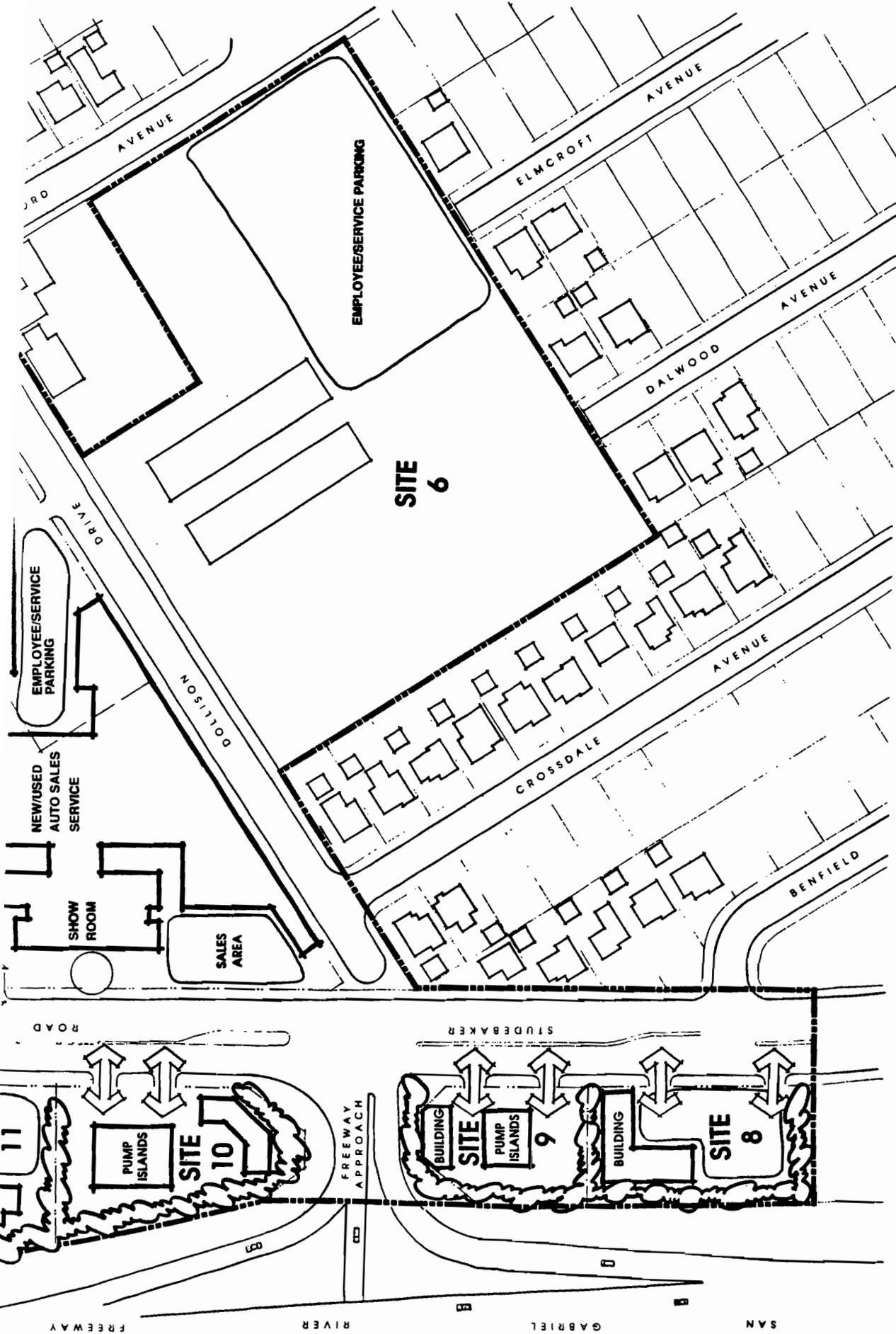
The specific plan envisions that the southbound I-5 ramp at Florence and Fairford may one day be vacated. Vehicles that now use this on-ramp would be diverted to the freeway on-ramps on Studebaker Road. Closure of this ramp is at the discretion of Caltrans, however, and is not expected to occur in the near future.

To prevent dealership-generated traffic (particularly test drives) from affecting the residential neighborhoods to the south, it is proposed that Dollison be vacated between Site 6 and Site 7. Crossdale/Dollison would be opened at the signalized intersection of Studebaker and the freeway ramps for neighborhood ingress/egress. Further, right turns onto Fairford from Dollison would be restricted. Such changes in circulation patterns would require approval of the City Engineering and Fire Departments.

## **F. Freeway Landscaping Concept**

The Santa Ana Freeway was constructed in the early 1950s and the San Gabriel River Freeway was completed in 1961. Much of the landscaping along those highways dates from that time and is now large and full. In the course of implementing this specific plan, the City of Downey may work with Caltrans to trim or replace some of this landscaping to allow better views of the proposed auto-related developments from the freeways. Such pruning and thinning should be avoided whenever possible, however, because existing vegetation partially buffers the residential neighborhood from freeway noise.





# FLORENCE AVENUE/ I-5 SPECIFIC PLAN

## City of Downey

### SITE PLAN CONCEPT



urban design studio

## **G. Freeway Sign Concept**

An oversized freestanding sign has identified the development on Site 1 (once Gemco, now Pace) since 1964. Because it is difficult to see the specific plan area from the freeways, this specific plan anticipates that a large sign will still be needed to attract customers to the area. The plan provides for replacement of the existing large sign—either on Site 1 or another site approved by the Planning Commission—with a sign that identifies the specific plan area and the major auto-related or large retail businesses within it.

## **H. Drainage Concept**

The storm drain system within the specific plan area is considered adequate, except in the event of a 100-year storm. Some improvements to storm drain systems may be required, based on the results of hydrology studies that the EIR requires for new developments. Further information about drainage is provided in Section VI of this document.

## **I. Water Distribution Concept**

The specific plan area is served by three water agencies, the City of Downey, the City of Santa Fe Springs, and Southern California Water Company. Existing water systems are expected to be adequate for domestic uses proposed for the study area under this specific plan, although water systems may need to be expanded to ensure adequate fire flow. The EIR calls for a future study of the water distribution system to evaluate possible redistribution of responsibilities among the three water purveyors. Further information about water is provided in Section VI of this document.



## **SECTION III – DEVELOPMENT STANDARDS**

In addition to those requirements contained within the Downey Municipal Code, the applicable minimum site development standards listed in this section shall apply. If the Downey Municipal Code conflicts with the following standards, the standards contained herein shall take precedence (except for restaurants and other retail commercial uses, which shall comply with the applicable Municipal Code standards).

### **SECTION III-A – STANDARDS APPLICABLE TO ALL SITES**

#### **A. Height**

The maximum height of buildings on each site, to top of ridge line, shall be as follows:

- |                               |                        |
|-------------------------------|------------------------|
| 1. Sites 1 and 5:             | sixty (60) feet        |
| 2. Sites 2, 3, 7, 11, and 12: | thirty-eight (38) feet |
| 3. Sites 4, 6, 8, 9, and 10:  | eighteen (18) feet     |

#### **B. Lot Area Requirement**

No property within the specific plan area may be subdivided into parcels less than one (1) acre in size. Existing parcels smaller than one acre may be developed according to the standards of this specific plan.

#### **C. Noise**

1. All body repair and compressor work, on those sites where such activity is permitted, shall be performed in fully enclosed areas only. Access doors to such enclosures shall be screened or shall face away from all public rights-of-way.
2. Air compressor exhaust stacks shall contain a muffling device.
3. Exterior loudspeakers shall be prohibited on all sites.
4. Air conditioning units and mechanical equipment may not be mounted on top of any building unless screened from view of all adjacent properties and public rights-of-way by screening involving an integrated architectural design element.

#### **D. Parking**

##### **1. Parking for Vehicle Dealerships**

Off-street parking spaces for vehicle dealerships shall be provided for customers and employees on the dealership site or within a common parking facility especially established for such purposes as follows:

- a. Each auto dealer shall provide a minimum of six (6) parking spaces reserved exclusively for customers. These spaces shall be readily accessible and shall be adjacent to the showrooms.
- b. Customer and employee parking shall be provided according to the following ratio:
  - One space for every 1000 square feet of floor area used for vehicle display and storage; plus
  - One space for every 400 square feet of floor area used for repair, service, parts, and supplies; plus
  - One space for every 250 square feet of floor area used for offices and administration.
- c. Employee parking facilities shared by two or more dealerships is encouraged. The requirements of Section 9150.32 of the Downey Municipal Code shall apply.
- d. On-site handicapped parking shall in accordance with the requirements of the Uniform Building Code.
- e. Parking of display and sales vehicles other than cars and trucks shall comply with Section 9150.06(d) of the Downey Municipal Code.

## **2. Parking for Other Uses**

Parking for all other uses shall comply with the provisions of Section 9150 of the Downey Municipal Code.

## **E. Right-of-Way and Streetscape Improvements**

Developer(s) of any site shall be responsible for improvements to public rights-of-way and streetscape adjacent to that site, including parkways and intersections.

## **F. Setbacks**

### **1. Applicability**

No structure of any kind, and no part thereof, shall be placed on any site closer to a property line than herein provided. The following structures and improvements are excluded from these setback provisions:

- a. Eaves, which may extend thirty (30) inches into a required yard.
- b. Landscaping and security devices such as bollards.
- c. Lighting
- d. Paving and curbing related to landscaped areas.
- e. Signs
- f. Steps and walkways, unroofed and unenclosed.
- g. Walls, except that no wall exceeding 42" in height shall be placed within landscaped or street setback areas.

## 2. Building Setbacks

- a. Dollison Drive, north side:
  - West of Fairford: One (1) foot  
(Established by Zone Exception 516, 1021, 1371)
  - East of Fairford: Ten (10) feet
- b. Dollison Drive, south side: Fifty-five (55) feet
- c. Fairford Avenue
  - North of Dollison, both sides: Ten (10) feet
  - South of Dollison, west side: Two hundred thirty-five (235) feet
- d. Florence Avenue, north side:
  - Auto Sales/Display Buildings: Sixty (60) feet
  - Service and Storage Buildings: One hundred (100) feet
- e. Florence Avenue, south side:
  - Auto Sales/Display Buildings: Forty (40) feet
  - Service and Storage Buildings: Eighty (80) feet
- f. Studebaker Road, both sides:
  - Auto Sales/Display Buildings: Forty (40) feet
  - Service and Storage Buildings: Eighty (80) feet
  - Gen. Commercial/Service Stations: Fifteen (15) feet
  - Service Station Pumps: Sixty (60) feet
- g. Freeways:
  - I-5 right-of-way: Sixty (60) feet
  - I-605 right-of-way: No setback required
- h. Interior Property Lines:
  - Adjacent to residential uses: Eighty (80) feet  
(See Section III-E for Site 6 requirements)
  - Adjacent to other uses: No setback required

## 3. Vehicle Setbacks

- a. Displayed vehicles and on-site parking shall be setback a minimum of three (3) feet from all street property lines, and a five (5) foot setback is preferred. The setback area shall be fully landscaped.
- b. Displayed trucks and recreational vehicles greater than seven (7) feet in height shall be setback a minimum of thirty (30) feet from street property lines.

## **G. Signs**

### **1. Purpose**

The purpose of these criteria is to guide businesses in selection and placement of signs. It is the intent of this specific plan to allow a reasonable number and size of signs, to foster continuity and appropriate scale, and to allow artistic flexibility. The sign guidelines in this specific plan are mandatory criteria to which each sign in the area must conform.

### **2. General Criteria**

- a. A comprehensive sign plan noting the type, size and location of all permanent signs shall be submitted concurrent with the architectural Design Review process for any development. No permanent signs shall be installed on the parcel before that property's sign plan has been approved.
- b. All signs shall be set back a minimum of five (5) feet from street property lines.
- c. For vehicle dealerships, the total area of all wall and monument signs shall not exceed one (1) square foot of sign area for each lineal foot of street frontage or 2.25 square feet of sign area for each lineal foot of building frontage, whichever is greater. Window price signs on individual cars shall not be computed as sign area.
- d. The face area of all monument and wall signs shall be computed according to the standards set forth in Section 9150 of the Downey Municipal Code.
- e. The maximum height of all signs shall be measured from the nearest curb grade or grade of the nearest ground-level (non-elevated) paving area, whichever is higher.
- f. No pennants, display flags, spinners, exposed lamps, banners, paper signs, painted signs, or other attention-getting devices may be permitted except for special events as governed by Section 9148.20(l) of the Downey Municipal Code.
- g. Exposed raceways, ballast boxes, or transformers shall not be permitted.
- h. Sign company names or stamps shall be concealed.
- i. Roof-mounted signs shall not be permitted.
- j. Sign colors should be coordinated with the development's overall color scheme. Signs should reflect high-quality design and should not use unusual materials, colors, or forms.

### **3. Freestanding Sign**

One freestanding sign will identify the specific plan area and the vehicle dealerships within it. This sign shall maximize visibility from the freeways, but shall in no case be higher than eighty (80) feet. A possible location for this sign is that used by the existing freestanding sign on Site 1. An alternative site, however, may be determined by the City Planning Commission.

### **4. Monument Signs**

- a. Signing within the specific plan area shall be by monument sign. Each parcel may have one monument sign for identification. Each monument sign may be double-sided and constructed of solid masonry or metal panels. Parcels occupied by automobile dealerships may be permitted one monument sign per franchise.
- b. Automobile dealer monument signs may contain the following information:
  - brand(s) of vehicles sold
  - manufacturer's logo
  - dealer's name
  - identification of used cars and/or trucks
  - secondary manufacturer's lines or similar identification
- c. Each sign face may not exceed eighty (80) square feet in area, nor exceed a height of eight (8) feet.
- d. Signs shall be located no closer than seventy-five (75) feet from a common lot line between dealerships and no closer than one hundred fifty (150) feet to other signs on the same side of the street.
- e. Monument signs shall be trimmed with a material used in the main building in order to promote an architecturally coordinated look for each site.
- f. Signs shall be contained within a landscaped area equal in size to the area of a single sign face. The minimum size of the landscaped planter, however, shall be eighty (80) square feet.

### **5. Wall Signs**

- a. All wall signs shall be internally back-lighted reverse channel lettering. Each sign may have no more than one letter color. Logos may include additional colors. Painted wall signs and internally-illuminated can signs will not be permitted.
- b. Wall signs may contain manufacturers' logos, brands of vehicles sold, and dealers' names.

- c. Letter sizes shall be proportional to the size of the mounting surface and shall not exceed:
  - twenty (20) inches for Sites 4 and 8
  - thirty (30) inches for all other sites
- d. Information may be displayed once per building face for each street or freeway orientation, up to a maximum of three sides of a building
- e. Informational wall signs identifying a dealership's service area or other specific functions may be permitted. Such signs shall not protrude out from the building or wall, shall be compatible with the building's design, and shall not exceed four (4) square feet.

## **6. Directional Signs**

- a. Directional signs may be approved to indicate a business' entrance, exit, service, and customer parking. Such signs shall not be computed as part of allowed sign area.
- b. Directional signs shall be internally-illuminated, double-sided monument signs only, and materials used shall be compatible with the business' building design and overall sign program.
- c. Directional signs shall not exceed six (6) square feet per sign face, nor have a sign face dimension greater than four (4) feet. The maximum height for a directional monument sign shall be three (3) feet.
- d. Directional signs shall be located no closer than five (5) feet to the street property line.

## **7. Service Station Signs**

Service station signs shall comply with Sections 9148.16(e)(3) and 9148.16(e)(4) of the Downey Municipal Code. Primary signs shall be monument signs only.

## **8. Signs for Other Uses**

Signs for uses other than those noted above shall comply with the Section 9148 of the Downey Municipal Code.

## **H. Trash Enclosures**

Trash enclosures shall not be permitted in any street setback area, and shall not be visible from public rights-of-way. Enclosures shall be constructed to the following standards.

### **1. Design**

Each Trash enclosure shall consist of seven (7) foot high walls designed and constructed to match the colors and materials of the building(s) it serves. Concrete runners, 4" x 4", shall be provided around the bottom of the interior. The enclosure shall be set on a 4"-thick concrete slab.

### **2. Gates**

All trash enclosures shall have garage door-type lift gate or swing gates of light-weight decorative plywood, aluminum or comparable material. An additional personnel access door shall also be provided. Both gates shall be closed at all times when not being used.

### **3. Lids**

All trash shall be placed and maintained in metal or plastic containers with overlapping, fly-tight lids. The lid shall be secured in place at all times when the container is not being filled or emptied. Enclosures visible from upper-story residences shall be screened by a trellis. Restaurant trash enclosures shall have self-closing lids. The tops of all enclosures shall be fitted with fine mesh screening.

### **4. Size**

Interior dimensions shall be at least one foot longer on all sides than the trash bin. The following is the minimum enclosure area, which may be provided at a single location or in several locations that equal the total area required.

a. Less than 5000 sq.ft.	50 sq.ft. enclosure
b. 5,000 to 19,000 sq.ft.	70 sq.ft. enclosure
c. 20,000 to 25,000 sq.ft.	84 sq.ft. enclosure
d. over 25,000 sq.ft.	140 sq.ft. enclosure

## **I. Unlisted Uses**

Interpretation of the permitted uses as described shall be governed by Section 9156 of the Downey Municipal Code. Determination of uses for the Specific Plan area shall be made at a noticed public hearing of the Planning Commission.

## **J. Utilities**

Utilities, including transformers, shall be installed underground for all new developments.

## **K. Vehicle Display Standards**

1. Vehicle display above the first level of a building shall be allowed only if oriented toward a freeway.
2. Trucks and recreational vehicles greater than seven (7) feet in height shall not be displayed within thirty (30) feet of a street property line.

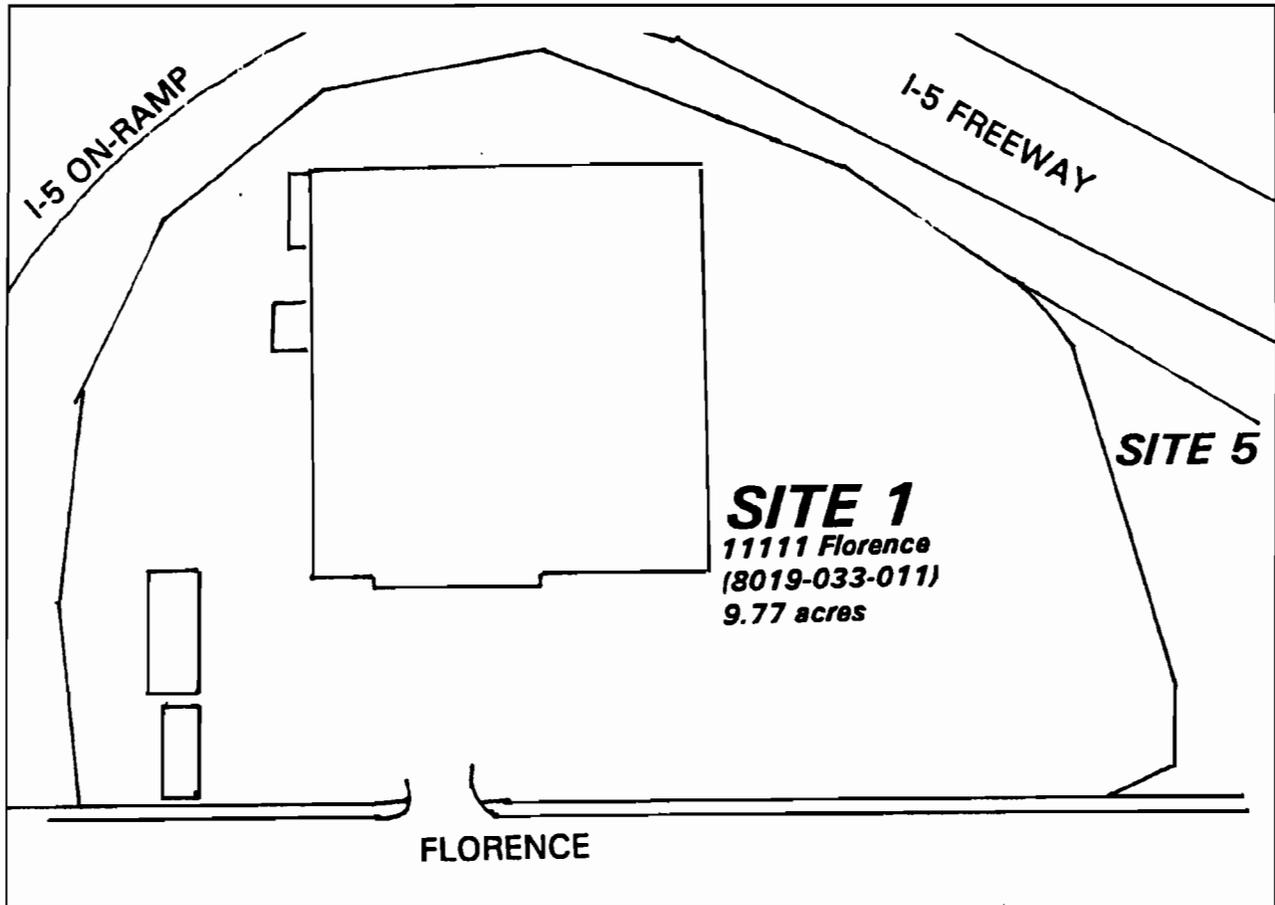
## **L. Walls and Fences**

1. Walls and fences shall not exceed:
  - a. Eight (8) feet in height on Site 6
  - b. Ten (10) feet in height for all other sites, except for walls on Sites 3 and 7, parallel to the Dollison Drive setback, which may be twenty (20) feet high.
2. In building setback areas, walls may not exceed 42" in height.
3. All walls shall be constructed of decorative masonry block, stucco, wrought iron, or a combination thereof.
4. Barbed wire shall not be allowed on top of any wall or fence.

## SECTION III-B: SITES 1 AND 5

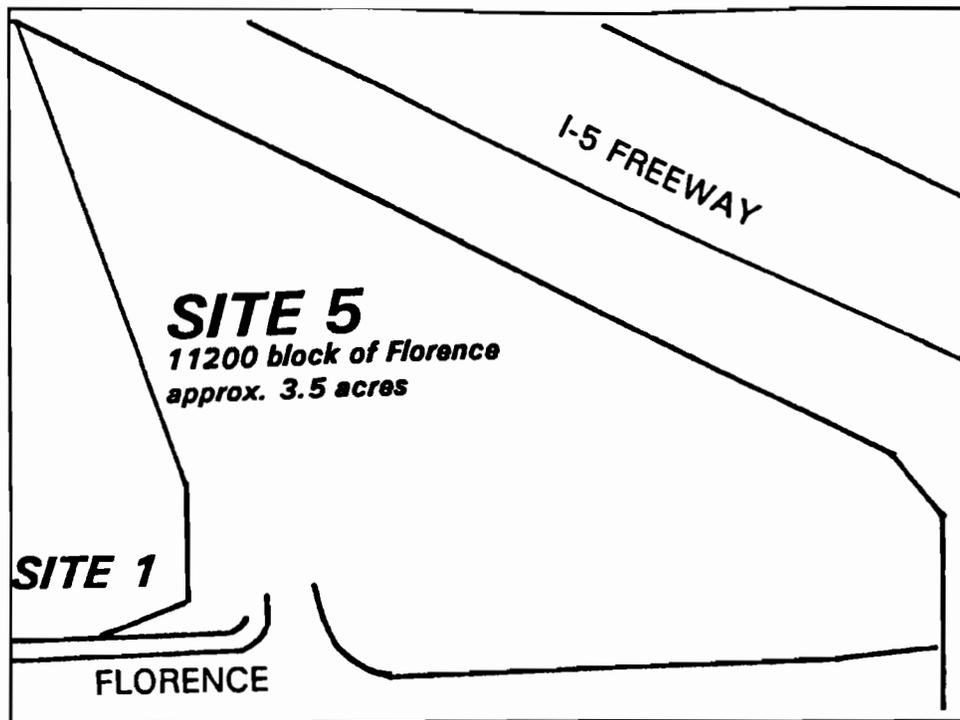
### A. Site Profiles

Site 1, at 9.77 acres, is the largest parcel in the study area and is currently developed with a Pace Membership Warehouse. The site is relatively flat and paved. The parcel has more than 900 feet of Florence Avenue frontage and is visible from Florence, Studebaker, and the Santa Ana (I-5) Freeway. If Caltrans vacates the freeway on-ramp and its property to the east (Site 5) at a future date, Site 1 could be expanded to include that area. To optimize the development potential of the area north of Florence, it would be ideal to combine Sites 1 and 5.



Site 5 is approximately 3.5 acres owned by Caltrans and used as an on-ramp for the southbound Santa Ana Freeway. Caltrans may declare this property surplus at some time in the future and sell it. For this reason, development standards are presented here for this parcel.

The preferred use for both Site 1 and Site 5 is auto sales.



## B. Land Uses

### 1. Principal Use

Dealerships oriented principally toward the sale of new passenger vehicles and light-weight trucks.

### 2. Conditional Uses

The following uses may be permitted subject to the approval of a conditional use permit, as provided in Section 9166 of the Downey Municipal Code:

- a. Auto body, repair, paint, and upholstery shops as a secondary part of operating a new vehicle dealership.
- b. General Merchandise Stores, including department stores, variety stores, retail lumber and building materials dealers, and general merchandise catalog show-rooms.
- c. Hotels
- d. Recreational vehicle sales (new vehicles only)
- e. Restaurants, including drive-in
- f. Specialty vehicle sales, including motorcycles and boats
- g. Supermarkets
- h. Used automobile sales (exclusively)

### **3. Accessory Uses**

As a secondary part of the operation of a new vehicle dealership, the following uses may be permitted:

- a. New vehicle storage and preparation
- b. Used vehicle sales, storage, and display
- c. Vehicle leasing
- d. Vehicle part and supply sales
- e. Vehicle repair and service
- f. Vehicle washing

### **4. Prohibited Uses**

- a. Outdoor uses not fully enclosed within a building other than vehicle sales/display and vehicle storage.
- b. Dismantling of vehicles or storage of vehicles for parts.

## **C. Site-Specific Development Standards**

### **1. Additional Standards for Site 1**

- a. Primary access shall be located at the intersection of Florence and Studebaker. Two other access points may be permitted along Florence Avenue.
- b. The major vehicular entry at Studebaker shall receive special design attention and enhancements.

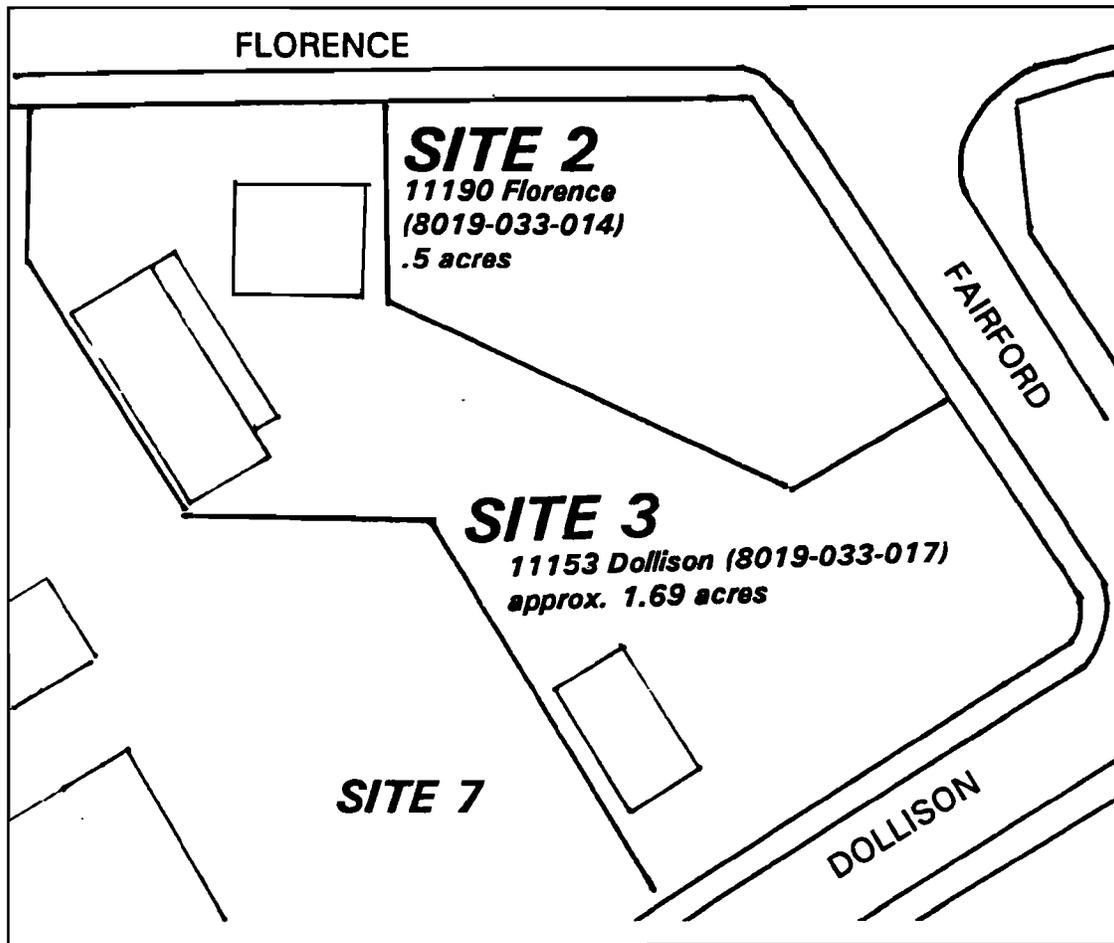
### **2. Additional Standard for Site 5**

Primary access to Site 5 shall be from Florence Avenue.

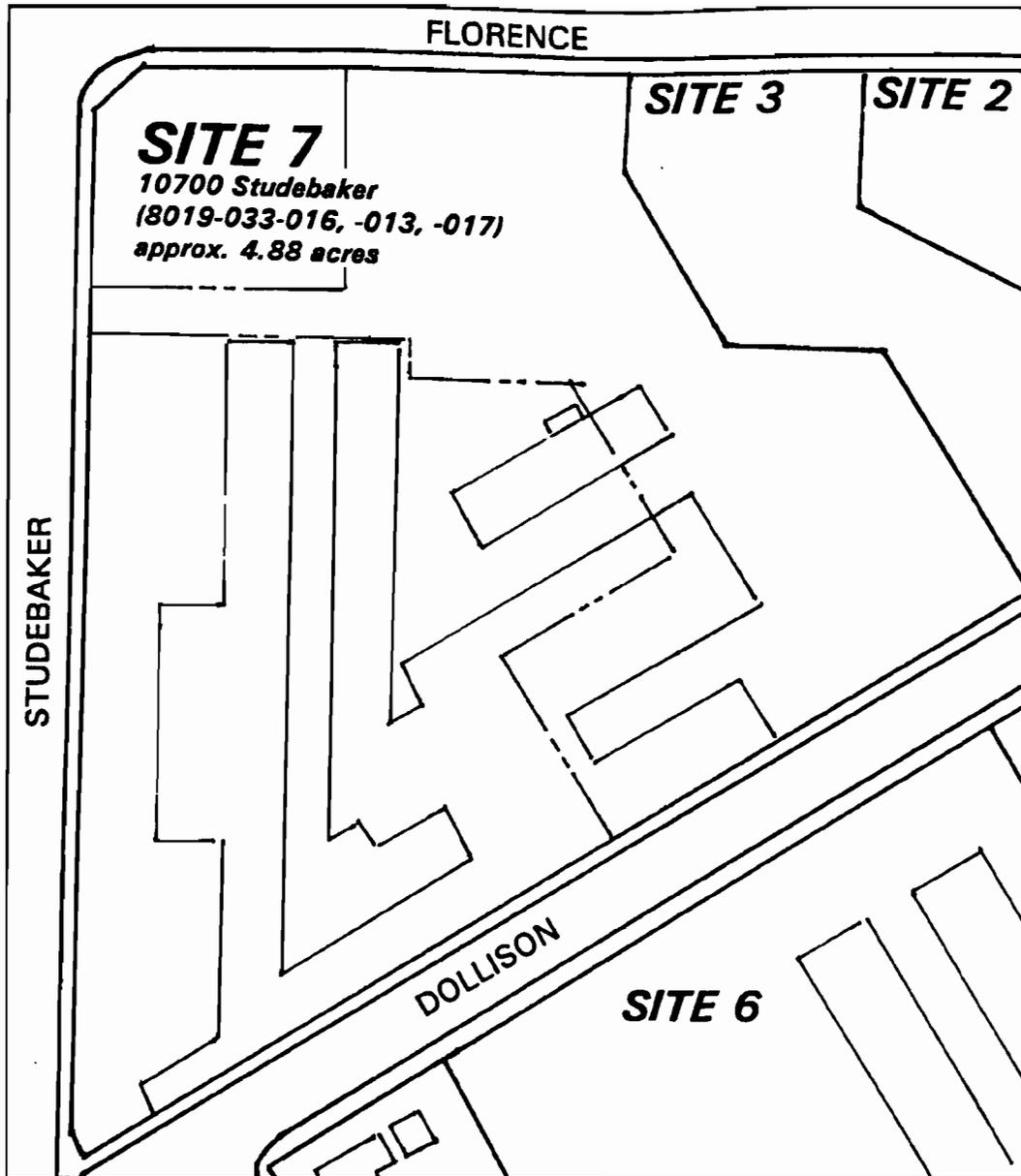
## **SECTION III-C: SITES 2, 3, 7, 11, AND 12**

### **A. Site Profiles**

**Site 2** is a half-acre parcel at the southwest corner of Florence and Fairford, currently used for automobile sales. The parcel is flat and its visibility to both west- and eastbound traffic on Florence is great. Visibility from the I-5 freeway is minimal. Combination of this parcel with Site 3 would create an opportunity for a larger development with a better layout and circulation pattern.



**Site 3** is an area of approximately 1.69 acres and is roughly half of a larger parcel of irregular shape. The other half of this property is occupied by the Penske Cadillac dealership and is considered in this specific plan as part of Site 7. A wall follows an irregular line that separates the halves. Site 3 is currently developed with a single-family residence and several old farm outbuildings, including an unused berry stand. Much of the parcel is used to store old tractors and other farm equipment. The house and outbuildings served the groves that once occupied most of the specific plan area, including Sites 1 and 7. The irregular line of Site 3's western boundary dates from that time. Site 3 has access to Florence, Fairford, and Dollison. This site could best be developed if it were combined with Site 2 to form a 2.19 acre parcel.

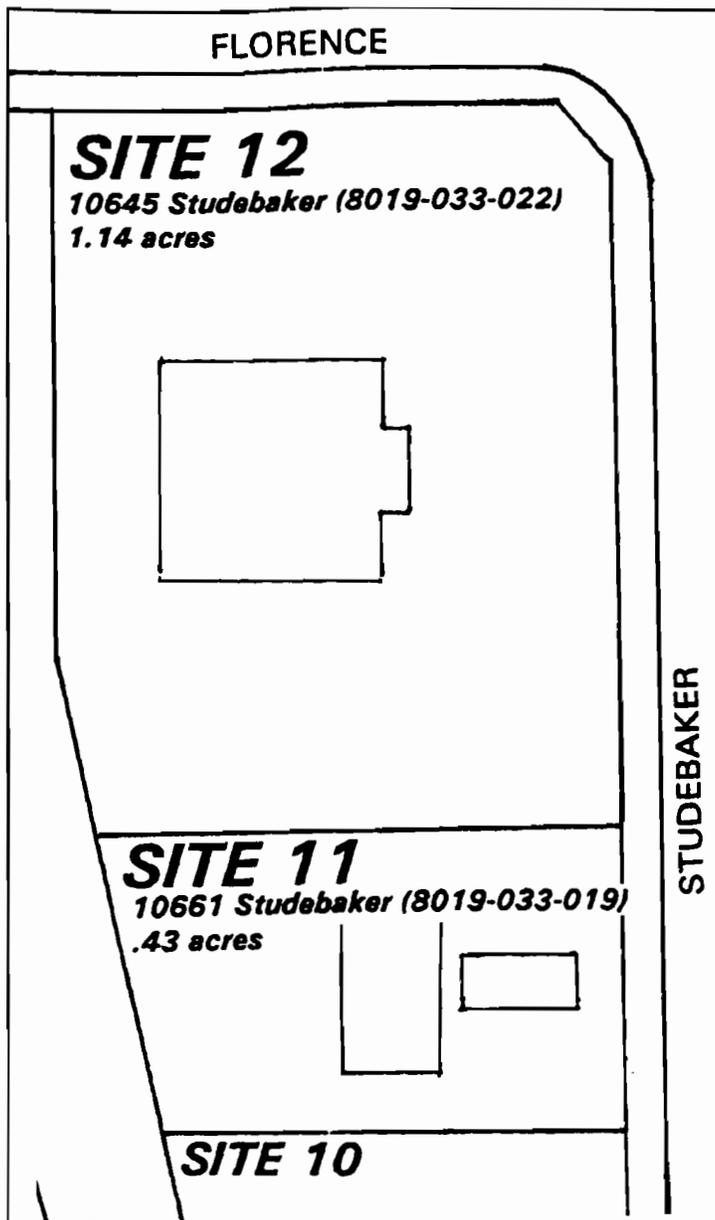


**Site 7**, approximately 4.88 acres, consists of two parcels of record and a portion of a third. The land is considered one site because it is occupied by the Penske Cadillac dealership, which holds the ground lease for the entire area. The site fronts the most prominent intersection in the specific plan area (Florence/Studebaker). The dealership is oriented toward Studebaker, where it has 605 feet of street frontage. Little of the business faces Florence, although the property fronts that street for more than 640 feet. The existing use is clean and attractive. The site is not readily visible from the freeways although its visual impact is great from both Florence and Studebaker.

Site 11 is a .43 acre parcel currently developed with a Union 76 service station. It fronts Studebaker Road for 110 feet. Penske Honda occupies the parcels (10 and 12) to the immediate north and south.

Site 12 is currently developed with a new vehicle dealership, Penske Honda. The site is at the prominent southwest corner of Florence and Studebaker, with 180 feet of frontage along the former and 250 feet along the latter. Penske Honda occupies two non-contiguous parcels in the specific plan area. The development potential and the use of this 1.14 acre site would be enhanced if Site 12 were combined with Site 11.

The preferred use for these sites is auto sales. It is preferred that Sites 2 and 3 be combined for a single development, and that Sites 11 and 12 also be combined.



## B. Land Uses

### 1. Principal Use

Dealerships oriented principally toward the sale of new passenger vehicles and light-weight trucks.

Automotive Service Station (Site 11 only)

### 2. Conditional Uses

The following uses may be permitted subject to the approval of a conditional use permit, as provided in Section 9166 of the Downey Municipal Code:

- a. Auto body, paint, and upholstery shops as a secondary part of operating a new vehicle dealership.
- b. Automotive Accessory sales
- c. Recreational vehicle sales (new vehicles only)

- d. Restaurants, including drive-in
- e. Used automobile sales (exclusively)

### **3. Accessory Uses**

As a secondary part of the operation of a new vehicle dealership, the following uses may be permitted:

- a. New vehicle storage and preparation
- b. Used vehicle sales, storage, and display
- c. Vehicle leasing
- d. Vehicle part and supply sales
- e. Vehicle repair and service
- f. Vehicle washing

### **4. Prohibited Uses**

- a. Outdoor uses not fully enclosed within a building other than vehicle sales/display and vehicle storage.
- b. Dismantling of vehicles or storage of vehicles for parts.
- c. Residential uses

## **C. Site-Specific Development Standards**

### **1. Additional Standards for Sites 2 and 3**

- a. Access to Site 2 shall be from Fairford only. Vehicles exiting onto Florence Avenue may interfere with traffic flow at the Florence/Fairford intersection.
- b. Primary access to Site 3 shall be from Florence Avenue. Once secondary access point may be permitted along Fairford.
- c. No use may be oriented toward Dollison, and no vehicular access shall be permitted from Dollison.
- d. No windows or doorways, other than employee entrances constructed to specifications of the Building and Safety Division, may open onto Dollison Drive.

**2. Additional Standards for Site 7**

- a. Primary vehicle access may be taken from either Florence Avenue or Studebaker Road. The preferred orientation for new dealerships or other uses is toward Florence Avenue. No use should be oriented toward Dollison Drive.
- b. Separate vehicle access is preferred for new car sales, service and used car sales.
- c. Special landscape treatment shall be incorporated at the Studebaker/Florence intersection, consistent with treatments on the other three corners.
- d. No windows or doorways, other than employee entrances constructed to specifications of the Building and Safety Division, may open onto Dollison Drive.
- e. The 78'8" freestanding sign on this property was permitted by Zone Variance 571 in 1988. According to the conditions of that approval, this sign will be allowed to remain "for as long as Penske operates at the site." Text changes must be approved by the Planning Commission.

**3. Additional Standards for Sites 11 and 12**

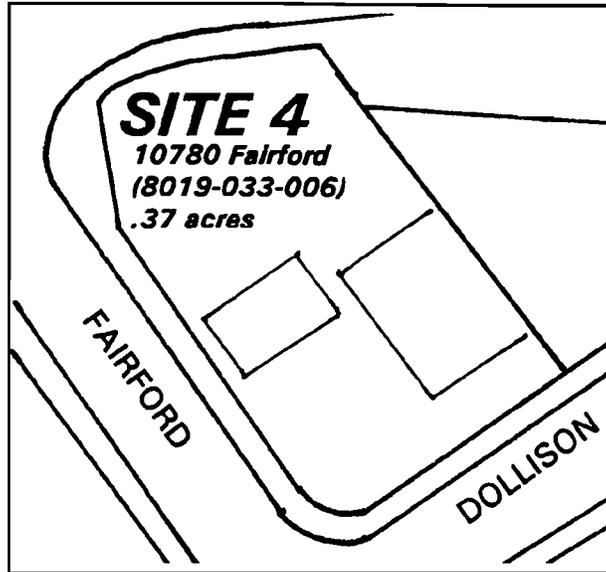
- a. If Site 11 is developed as a service station, the standards set forth in Section III-G of this specific plan shall apply.
- b. Vehicular access shall be from Studebaker Road only.
- c. Special landscape treatment shall be incorporated at the Studebaker/Florence intersection, consistent with treatments on the other three corners.
- d. A buffer consisting of landscaping or a wall shall be located between Sites 10 and 11, unless the sites are merged.

## **SECTION III-D: SITE 4**

### **A. Site Profile**

Site 4 is a .37 acre parcel at the southeast corner of Florence and Fairford, currently developed with an auto repair facility. As the smallest site in the specific plan area, its development is most constrained by required parking standards. The site, however, is highly visible, particularly to eastbound traffic on Florence Avenue.

The preferred use for Site 4 is automotive accessory retail.



### **B. Land Uses**

#### **1. Principal Uses**

- a. Automotive accessory retail
- b. Office uses including: employment agencies, insurance offices and title companies, investment service offices, legal offices, post office contract stations, professional offices as defined in Section 9120.04(f) of the Downey Municipal Code, real estate offices, and travel agencies.
- c. Medical and dental offices, related health services, and laboratories, the latter as an ancillary service only, not involving manufacture, fabrication, or sale of any article or commodity other than those incidental to the services provided.

#### **2. Conditional Uses**

The following uses may be permitted subject to the approval of a conditional use permit, as provided in Section 9166 of the Downey Municipal Code:

- a. Convenience food stores
- b. Restaurants, excluding drive-in

### **3. Prohibited Uses**

- a. Outdoor uses not fully enclosed within a building.
- b. Dismantling of vehicles or storage of vehicles for parts.
- c. Sales and/or installation of car stereo equipment.

### **C. Additional Standards for Site 4**

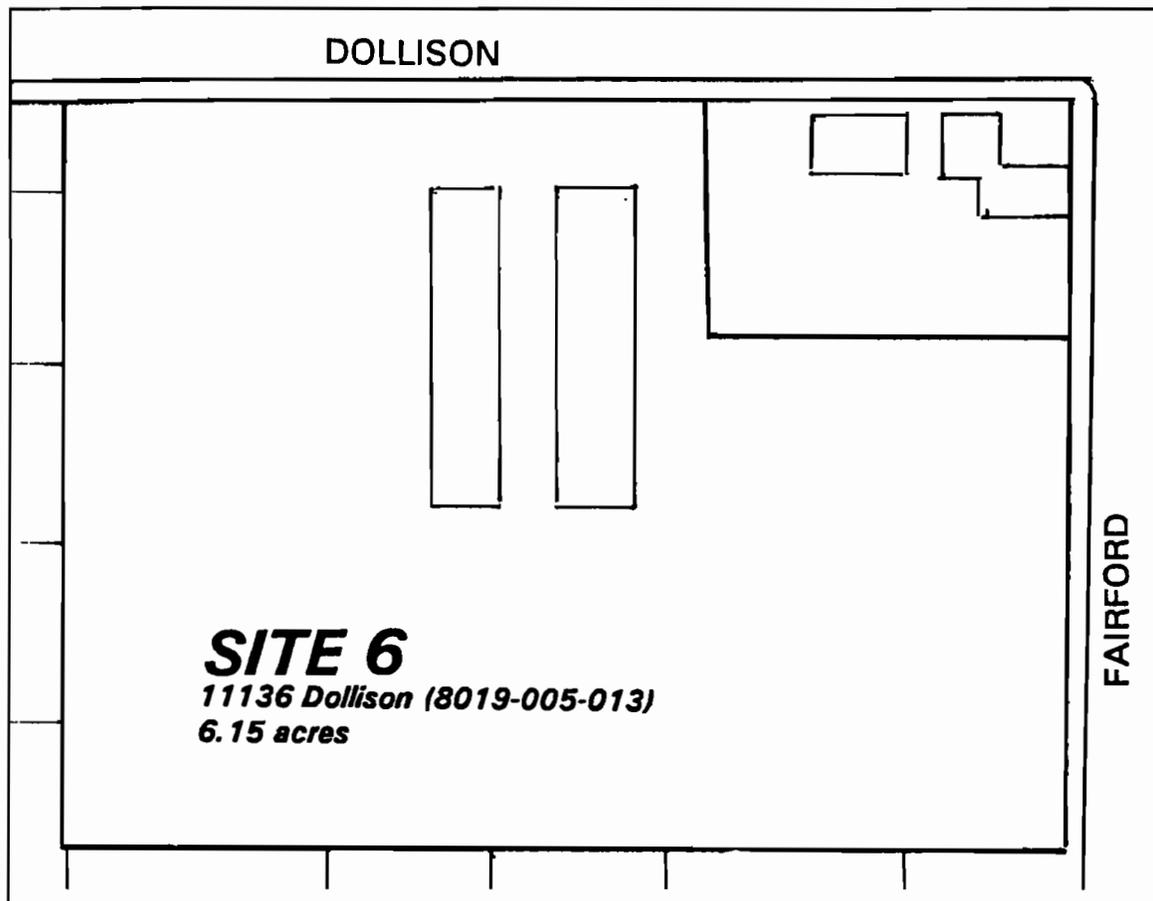
1. The site's access shall be from Fairford Avenue. Businesses may not be oriented toward Dollison Drive.
2. A decorative masonry wall shall be located parallel to Dollison and north of the 5' landscaped setback to protect and screen the adjacent residential area.
3. Lighting standards may be located no closer than sixty (60) feet to Dollison Drive and shall have no more than two (2) fixtures per pole.
4. Public telephones may be located on the site, but must be within or adjacent to the primary structure in a well-lighted area. Public telephones will not be permitted at the intersection of Fairford and Dollison.

## SECTION III-E: SITE 6

### A. Site Profile

Site 6 is a 6.1 acre developed parcel, currently used as the Service Department for Penske Honda and for service and storage for Penske Honda and Cadillac. These uses were permitted by Conditional Use Permits #211 (1984) and #248 (1987), and the development was designed to be as sensitive as possible to the residential uses that adjoin Site 6 on three sides. Structures and auto-related uses have been permitted only on a 1.3 acre central area, and this Specific Plan will continue this restriction. The site is not visible from Florence Avenue or from either freeway.

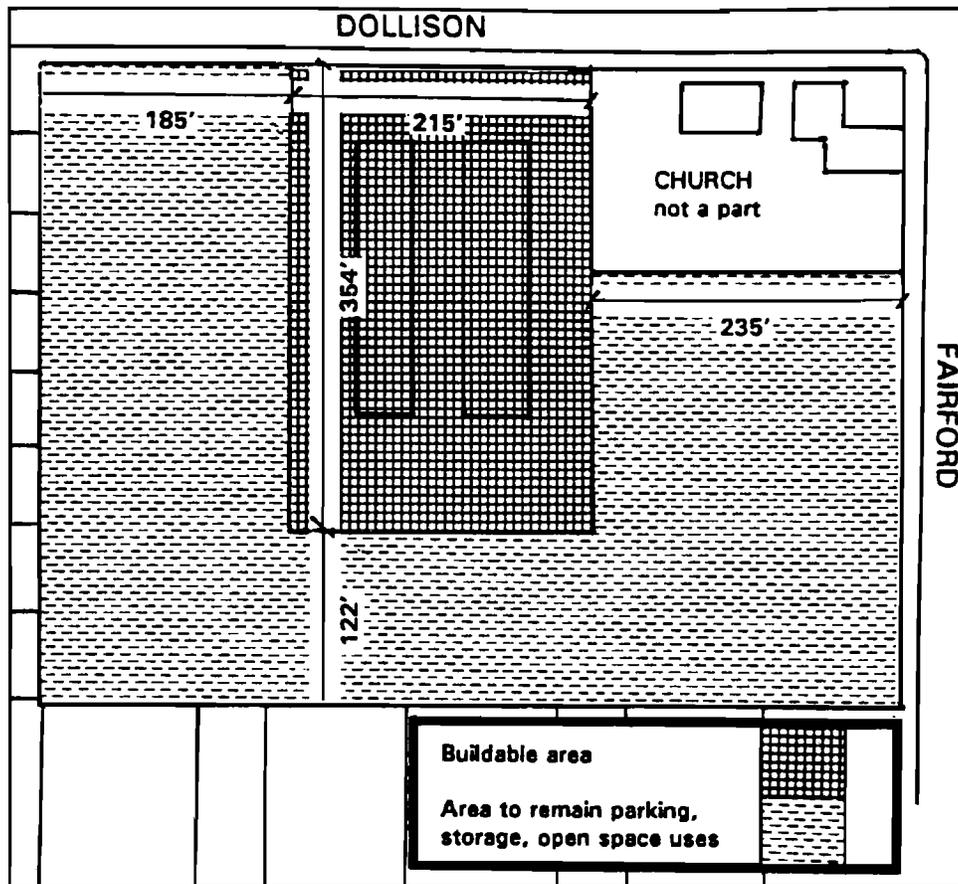
The preferred use for Site 6 is its current use—automobile service and storage.



### B. Land Uses

Zoning in effect for this parcel prior to the adoption of this specific plan allowed General Commercial (C-2) uses only on that portion of the site, 215' wide by 354.5' deep, immediately west of the church at 11156 Dollison. This area is shown by the checkered pattern on the exhibit below and is referred to in this specific plan as the Central Area. The remaining land (approximately 4.9 acres) was zoned OS-AP (Open Space-Auto Parking) to provide a buffer between the commercial and surrounding residential uses.

This portion of the site is referred to in this document as the Outer Area, and is shaded in the exhibit below with dashed lines.



It is the intent of Specific Plan 90-1 to retain this buffering of land uses, and to include standards for the 4.9-acre buffer area that emulate the restrictions of the OS-AP zone. Therefore, all uses listed below are permitted in the 215' x 354.5' Central Area. Only those marked with an asterisk (\*) are permitted in the Outer Area.

**1. Principal Uses**

Vehicle repair, parts sales, \*storage, and service as a secondary part of operating a new vehicle dealership.

**2. Conditional Uses**

The following uses may be permitted subject to the approval of a conditional use permit, as provided in Section 9166 of the Downey Municipal Code:

- a. \*Public and private parking lots
- b. Medical or professional offices.
- c. Any use permitted in the Central Area of Site 6, may be permitted with a conditional use permit in the Outer Area of Site 6, if all of the following conditions are met.
  - i. The proposed use is contained entirely within 50' of the Central Area and in no cases within 100' of the eastern, western, or southern property line.
  - ii. The use is incidental to the use on the Central Area.

- iii. Both the Central and Outer Areas remain part of the same parcel in undivided ownership.

### **3. Accessory Uses**

As a secondary part of the operation of a new vehicle dealership, the following uses may be permitted:

- a. \*Employee parking
- b. \*Recreational vehicle storage
- c. Vehicle leasing
- d. Vehicle part and supply sales
- e. Vehicle washing

### **4. Prohibited Uses**

- a. Outdoor uses not fully enclosed within a building other than vehicle sales/display and vehicle storage.
- b. Dismantling of vehicles or storage of vehicles for parts.

## **C. Additional Standards for Site 6**

1. Those uses listed in this section not marked with an asterisk shall be permitted only on that portion of the site described by the following legal description:

Beginning at a point in the center line of Dollison Drive (formerly Little Lake Road) 55 feet wide, as shown on the map of Tract 16589 as per map recorded in Book 406, Pages 8 through 10 inclusive, of maps in the Office of the County Recorder of said County, that is north 89°35'25" east 188.03 feet from the intersection of the northerly prolongation of the easterly line of said Tract 16589 and said center line of Dollison Drive; thence south 0°15'15" west 382.00 feet; thence North 89°31'25" east 215.00 feet; thence North 0°15'09" west 382.00 feet to said center line of Dollison Drive; thence south 89°31'25" west thereon to the point of beginning.

2. The site's access shall continue to be from Dollison Drive. There shall be no access to Fairford Avenue.
3. Monument signs on Site 6 may not exceed four (4) feet in height and their faces may not exceed thirty (30) square feet.
4. Primary identification wall signs may be installed only on building elevations facing Dollison Drive and on the outside of the perimeter wall facing Dollison.
5. Eight (8) foot high opaque walls shall continue to surround the site. The wall nearest and parallel to Fairford Avenue shall be set back a minimum of twenty (20) feet.
6. Site 6 could conceivably serve several users. The present configuration, however, is desirable. Subdivisions will not be acceptable unless specific measures are presented that will protect surrounding residential properties.

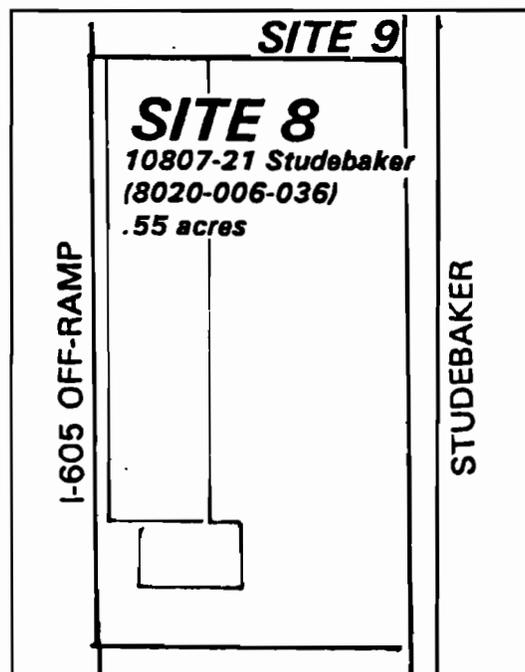
## SECTION III-F: SITE 8

### A. Site Profile

The southernmost parcel in the specific plan area, this half-acre property has limited development potential due to its size and its location adjacent to an apartment complex. Site 8 is currently developed with a five-tenant neighborhood commercial center. This is the best use of this property, given the site's location, lack of visibility, and limited access.

The commercial uses were originally permitted under Zone Exception case 1000 in 1972, which stipulated as a condition of approval that business operations may not extend past midnight. The Planning Commission upheld this condition in 1976 when it denied Zone Exception Case 1310, which requested permission to establish a 24-hour convenience market. The time limitation was imposed to protect the apartments to the south and the nearby detached houses from late-night noise and traffic. This time restriction will continue under this specific plan. The provisions of this specific plan shall supersede the requirements and conditions of Zone Exception 1000.

The preferred use for this site is neighborhood commercial.



### B. Land Uses

#### 1. Principal Uses

- a. Automotive accessory retail.
- b. Neighborhood Commercial uses as defined by Section 9122.04 (C-1) of the Downey Municipal Code, except for (ak) Liquor, on-sale, including beer and wine. Section 9122.04 is reproduced as Appendix A of this document.

- c. Employment agencies.
- d. Videotape rentals and sales.

## **2. Conditional Uses**

The following uses may be permitted subject to the approval of a conditional use permit, as provided in Section 9166 of the Downey Municipal Code:

- a. Public and private parking lots
- b. Restaurants, excluding drive-in
- c. Specialty vehicle sales, including motorcycles and jet skis, when totally enclosed within a structure.

## **3. Prohibited Uses**

- a. Any use, including those listed above, whose hours of operation extend past midnight.
- b. Outdoor uses not fully enclosed within a building other than vehicle sales/display and vehicle storage.
- c. Dismantling of vehicles or storage of vehicles for parts.
- d. Auto body, paint, and upholstery shops.
- e. Automotive repair of any kind.
- f. Bars or cocktail lounges.
- g. Sales and/or installation of car stereo equipment.

## **C. Additional Standards for Site 8**

- 1. The site shall continue to provide a wall or landscape buffer at the southern property line.
- 2. No more than two points of access to the site shall be allowed.
- 3. A 46' setback must be maintained from the residential property for future developments.
- 4. Most of the building mass of future development should be placed as close to the northern property line as possible to create a parking buffer area between the commercial uses and the apartments to the south.
- 5. New multi-tenant development should be L-shaped. Multi-tenant centers with a single setback should be avoided. A minimum of 10% of the building shall be located at the fifteen (15) foot street setback line.
- 6. Businesses shall not operate between the hours of midnight and 6:00 a.m.

## SECTION III-G: SITES 9 and 10

### A. Site Profiles

Site 9 is a half-acre parcel located on the southern corner of Studebaker Road and the Freeway on/off ramps. The site has approximately 170 feet of frontage on Studebaker and is currently occupied by a service station.

Site 10 is a .7 acre parcel on the same side of Studebaker as Site 9, but on the north side of the freeway on/off ramps. It is currently used for auto sales.

The preferred use for both sites is automotive service station.

### B. Land Uses

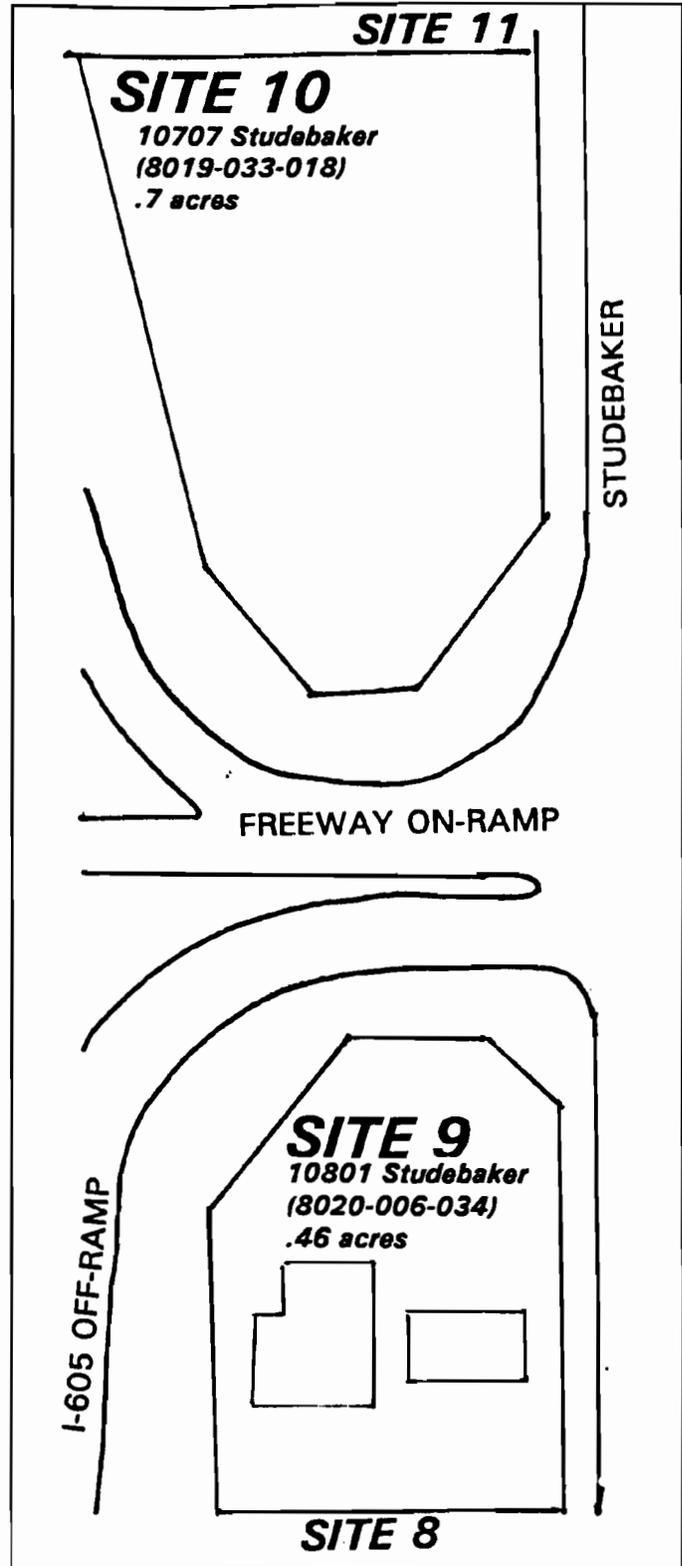
#### 1. Principal Uses

- a. Automotive service station
- b. Automotive maintenance (e.g., SmogPros, JiffyLube)
- c. Car wash
- d. Automotive accessory retail (Site 9 only)
- e. New and used auto sales (Site 10 only). If auto sales is chosen as the use for Site 10, the development standards contained in Section III-C for Site 11 shall apply.

#### 2. Conditional Uses

The following uses may be permitted subject to the approval of a conditional use permit, as provided in Section 9166 of the Downey Municipal Code:

- a. Convenience markets, as an accessory use to service stations
- b. Restaurants, including drive-in



### **3. Prohibited Uses**

- a. Automotive repair with outdoor storage.
- b. Dismantling of vehicles or storage of vehicles for parts.
- c. Outdoor storage of any materials.

### **C. Additional Standards for Sites 9 and 10**

1. Primary access shall be at least fifty (50) feet from the on/off ramps to facilitate traffic movement.
2. All new or substantially rehabilitated service stations shall be designed so that the main building is between the pump aisles and the freeway approach ramps.
3. No overnight outdoor storage shall be permitted. All stored materials, autos, goods, and other articles must be within a fully enclosed building.
4. Lighting fixtures shall not exceed a height of eighteen (18) feet.
5. Uplighting shall be used from ground level and from reveals along buildings, walls, and pump islands.
6. Service stations may devote no more than twenty (20) percent of their retail floor area to convenience food sales with a conditional use permit. Retail floor area excludes service bays and accessory structures. No alcoholic beverages shall be sold. Convenience markets may not operate between the hours of midnight and 6:00 a.m.
7. Storage and handling of hazardous materials such as gasoline, motor oil, solvents, and anti-freeze shall be in accordance with the provisions of the Hazardous Materials Management Element of the City's General Plan.

## **SECTION IV — DESIGN PRINCIPLES AND GUIDELINES**

### **A. Applicability**

The guidelines in this section apply to all new construction and substantial renovation of automotive dealerships, restaurants, service stations and other commercial uses within the specific plan area. Substantial renovation shall include facade changes and rehabilitation exceeding 50% of a structure's reasonable replacement value.

### **B. Purpose**

The purpose of these guidelines is to provide comprehensive design objectives for all land uses, particularly automotive sales and service centers, within the specific plan area. The guidelines are intended as steps to achieve the following goals:

- Attain a cohesive and friendly environment for display, sales, and service of motor vehicles.
- Enhance the efficiency, safety and visual effect of automobile dealerships.
- Establish an architectural theme that allows flexibility of design yet fosters a sense of architectural unity.

The guidelines are intended for use by developers, architects, planners, landscape architects and engineers. The photographs, sketches, and graphics in this section are presented only to illustrate the basic intent of the guidelines. They are not intended to depict, represent, or prescribe any actual development or building design.

The guidelines in this section cover four areas:

- **Site Planning**, including principles for creating efficient, safe, and visually pleasing layout of developments. This section addresses access, parking, setbacks, building location, display and sales areas, and storage and screening.
- **Architecture**. These guidelines cover height and bulk of structures, wall and roof articulation, architectural elements (i.e., windows, loading doors screen walls, etc.) building materials and colors, lighting, signs, and other architectural concerns.
- **Landscaping, Lighting, and Maintenance**. These guidelines govern landscape and lighting that softens and enhances sales and display areas, building architecture, and screenwalls.
- **Streetscape Improvements**, intended to contribute to the overall design theme and sense of place. These include:
  - Special paving materials and landscaping accents at the gateway intersections of Studebaker Road/Freeway ramps, Studebaker/Florence, and Florence/Fairford.
  - Spacious sidewalks and street trees along both sides of Florence and Studebaker to unify the area, to evoke a quality image for the auto dealerships, and to encourage pedestrian circulation between them.

## C. Site Planning Guidelines

The site planning guidelines are intended to promote developments that 1) have a clear identity and sense of place, 2) are safe and efficient, and 3) provide a harmonious and pleasing environment for all activities. The site planning criteria encourage coordinated development identity and high level of design quality while providing flexibility to maximize the character of individual projects. These criteria are formulated to provide qualitative guidance on how building location, design, parking, and landscaping may positively affect a development's visual quality.

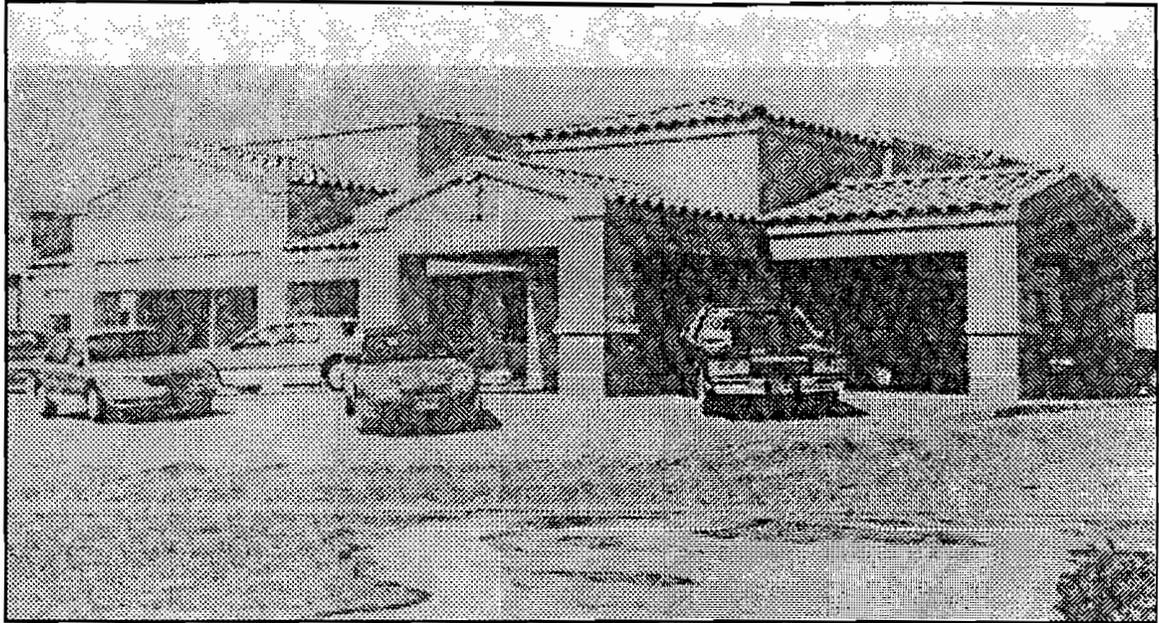


*Good site planning can create a unified appearance.*

### 1. General Criteria

- a. Buildings should be sited in a manner that will complement adjacent buildings, provide order and avoid a jumbled, confused appearance.
- b. Locate uses on a site to avoid conflicts with adjacent uses. Acknowledge concerns of residential and other non-automotive uses, and consider the topographic relationship of adjacent freeways and streets.
- c. Consider setbacks of adjacent properties. While articulation of building edges adds interest to the streetscape, the street should function as whole and the setbacks should be related.
- d. Single-story buildings should be on a raised pad, so that they will be visible above vehicles displayed or parked in the setback area.

- e. Outdoor sales areas should not appear as mere extensions of service and storage areas or other dealership activities. This separation or delineation may be accomplished with landscaping, hardscape (walkways, decorative drives), screenwalls, display areas or pads, spacing and location of vehicles, etc.
- f. Consider using simply designed display pads, podiums, terraces, pavilions, and showroom aprons rather than large, unarticulated sales lots for vehicle display.

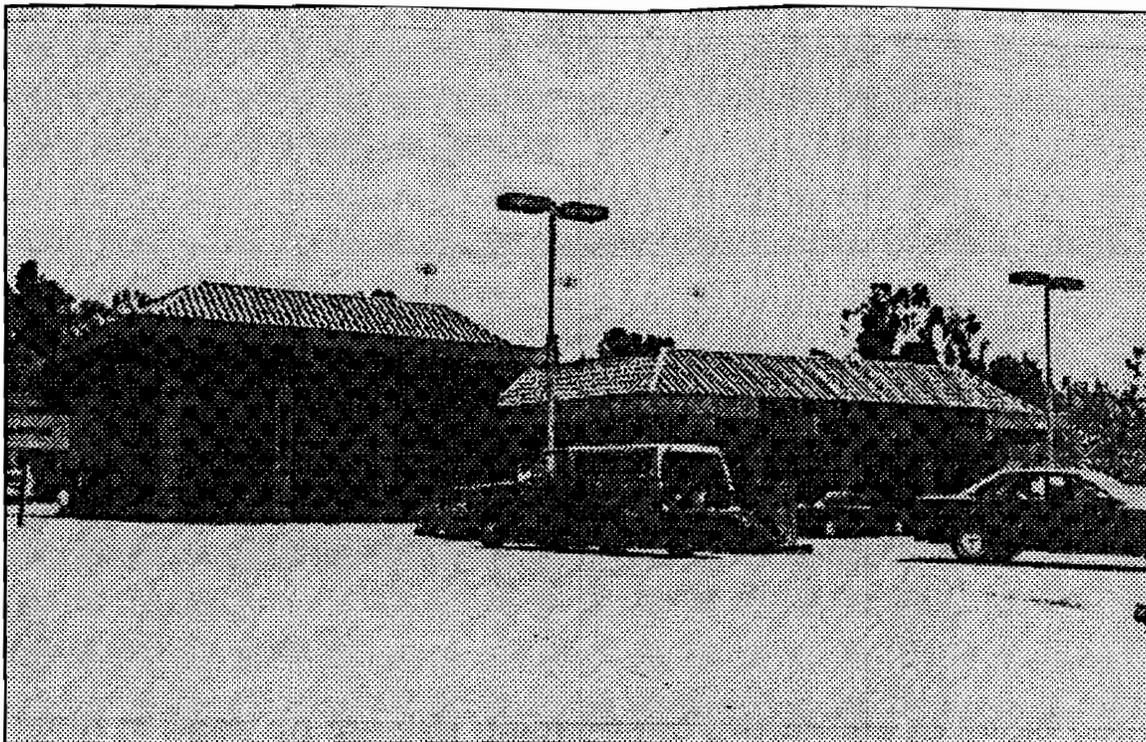


*Use simple display pads and pavilions.*

## **2. Parking, Storage, and Access**

The layout of parking areas for sales and service as well as for display and storage areas are critical to a development's visual impact, internal efficiency, and effect on adjacent properties, streets, and freeways. See Section III of this specific plan for regulations pertaining to parking, storage and display of vehicles.

- a. Access to loading and service areas should be separated from access to customer parking.
- b. Rear or side street access for service areas is preferred over principal frontage access.
- c. Separate employee and customer parking wherever possible. Customer service parking should also be separate from customer sales parking.
- d. Screen loading ramps and docks from view of all rights-of-way and adjoining properties, including freeways, with walls and landscaping.



*Locate service entrances to the rear of dealerships.*

- e. Park trucks and service vehicles only in screened storage areas. No storage shall be permitted between a principal frontage street and a building.
- f. Separate parking areas from buildings with raised concrete walkways or landscaped strips, preferably both. Avoid creating parking spaces that directly abut buildings.
- g. Shared facilities between dealerships are encouraged for such purposes as fuel storage and dispensing, truck unloading, and employee parking. Shared access between adjacent auto dealerships is also encouraged.
- h. Use walls, berms, and landscaping to screen and soften the view of large parking and storage areas from adjacent streets or freeways.
- i. Design all parking facilities so that vehicles may exit the property travelling in a forward direction.
- j. Driveway entry throats should be at least 25 feet wide (30-35 feet along Florence and Studebaker), and should be unimpeded by parking for at least 10 to 15 feet inside the property line to prevent conflict between vehicles entering and exiting the site.



*Wide driveways facilitate access.*

## **D. Architectural Guidelines**

### **1. Architectural Style**

#### **a. Selection Criteria**

An acceptable architectural style is meant to fulfill three objectives for the specific plan area. The first is establishment of a sense of place for automobile dealerships and other businesses. It is the intent that the style create a sense of character and consistency within the area. This can best be accomplished by choosing a particular architectural style within which all structures will be designed. This consistency is meant not to be constrictive nor to create a situation where all buildings look the same, but rather to allow diversity within stated parameters.

The second objective is to respond to the specific plan area's physical location along Florence, Studebaker, and the two freeways. The selected architectural treatment must create visual interest, screen objectionable views, and respond to the topography of each site and its adjacent properties.

The third objective is to respond to the variety of uses dealerships entail. The style should be simple in form, with frugal use of detail so that undue hardship is not placed on builders. The use of high-quality, durable materials that require low maintenance should be emphasized throughout all development projects.

#### **b. Preferred Architectural Style**

The architectural style which best fulfills these objectives is a style which for the purposes of these guidelines we will call Early California. This style incorporates elements of Mediterranean, Spanish Colonial, and Mission Revival architecture, is simple in form and materials, and can incorporate low-maintenance materials. It displays frugal use of detailing and adapts itself well to the variety of building types necessary for vehicle dealerships. It also blends easily with other architectural styles found within Downey and the region, particularly in other recently-constructed dealerships in the City such as Downey Toyota and Downey Acura.

This section defines the general architectural character and elements to be incorporated into the Early California style. Photographs and graphics are used to convey an understanding of the desired style where words alone may not be sufficient. These representations do not depict any particular project or design application.

#### **c. Alternate Architectural Styles**

Design continuity between properties is a primary goal of this specific plan. A developer or property owner may wish to construct a project within the study area using a style that does not incorporate the Early California style but is similar to the style used on an existing adjacent parcel. This may be permitted.

The Design Review Board shall determine whether a proposed development sufficiently coordinates with an existing development adjacent to or across the street from the subject property. All other provisions of this Section (i.e., site planning, equipment screening, landscaping requirements, streetscape improvements, etc.) shall apply.

The Design Review Board, and Planning Commission and City Council on appeal, may approve proposals employing other architectural styles, if the Board finds that the project demonstrates particular excellence and merit in design. In approving projects not employing the Early California Style, the Design Review Board shall make particular note of the following two findings required by Section 9162.12 of the Downey Municipal Code for design review:

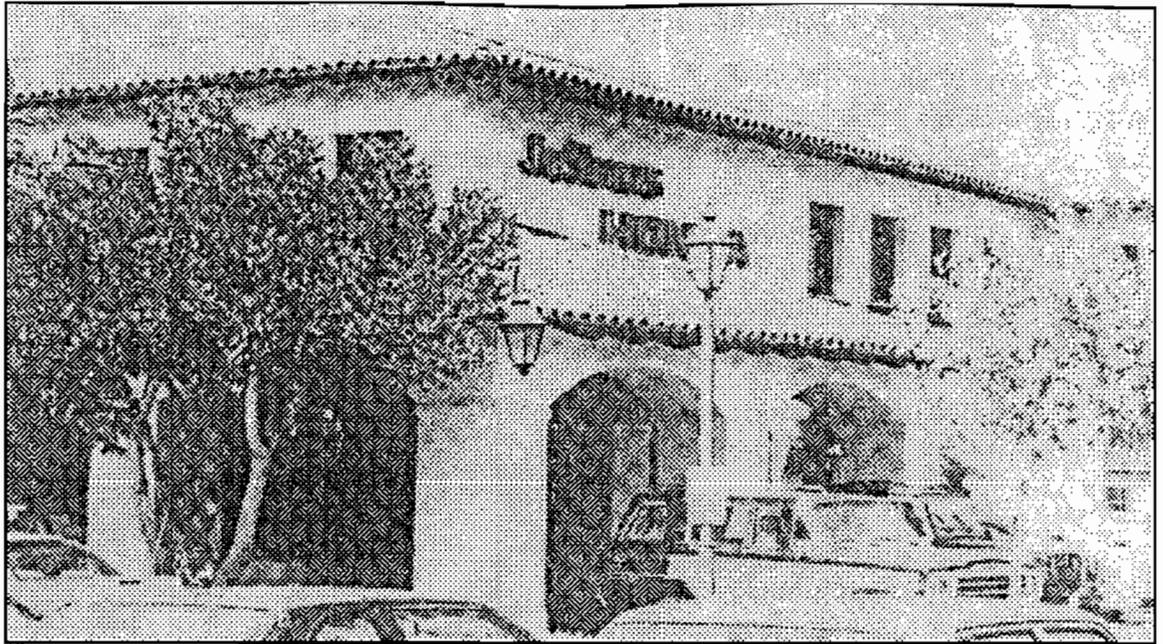
(a) That the proposed development's site plan and its design features will integrate harmoniously and enhance the character and design of both the immediate neighborhood and the surrounding areas of the City.

(c) That the site plan and design will improve community appearances by preventing extremes of dissimilarity or monotony in new construction or in alterations of facilities.

**d. Architectural Imagery of the Early California Style**

The architectural images of the Early California style include strong unifying elements such as tile roofs, simple pale washed walls, uncluttered detailing, and angled and varied building perspectives. The simplicity of color and the play of light and shadowed walls are also important factors. Elements that form this distinctive style include:

- soft pastel building wall colors
- stucco finish with a variety of smooth textures
- simple pitched red barrel tile roofs
- open rafter tails with large overhangs
- the appearance of thick walls
- tile details
- deep-set window and door openings
- offset wall planes
- building masses with the incorporation of one and two story architecture
- sequencing of enclosed space/arches
- iron work



*Example of desirable architectural image.*

Elements to avoid or minimize are:

- dark or white wall colors
- unfinished concrete wall surfaces
- box-like contemporary architecture without articulation
- rustic or "woody" architecture
- wood siding
- wood fences
- gambrel, high-pitched, or extensive flat roofs
- slumpstone walls
- prefabricated metal
- "Corten" steel

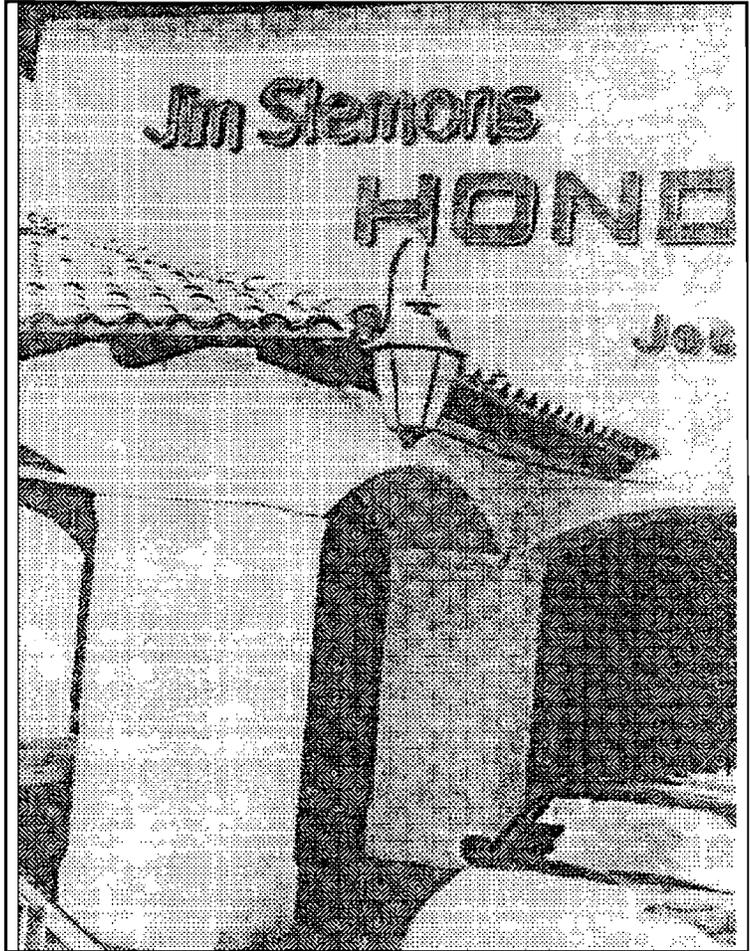
## 2. Materials, Massing, and Color

### Materials

Materials used in the Early California style are simple and straightforward: plaster, clay tile, masonry, wrought iron, and wood. Stucco is the predominant material used to cover the walls of buildings. All exterior building walls should be stuccoed or plastered.

Wood can be used as posts, beams, handrails, spindles, balcony floors on cantilever balconies, window grilles, vents, windows, shutters, and doors.

Clay or concrete tile shall almost always be used as a roofing material, patio paver, and wall cap. Traditional red barrel tile is preferred for roofs. Masonry can be used in the form of carved stone or cast concrete as ornamental door and window surrounds, wall caps, fountains, and columns.



*Appropriate materials for Early California style.*

Wrought iron is appropriately used for handrails, window grilles, gates, lighting fixtures, signs, and door and shutter hardware.

Ceramic tile can be used on the exterior for door and window surrounds, wainscots on walls, wall fountains, and stair risers.

### **Appropriate**

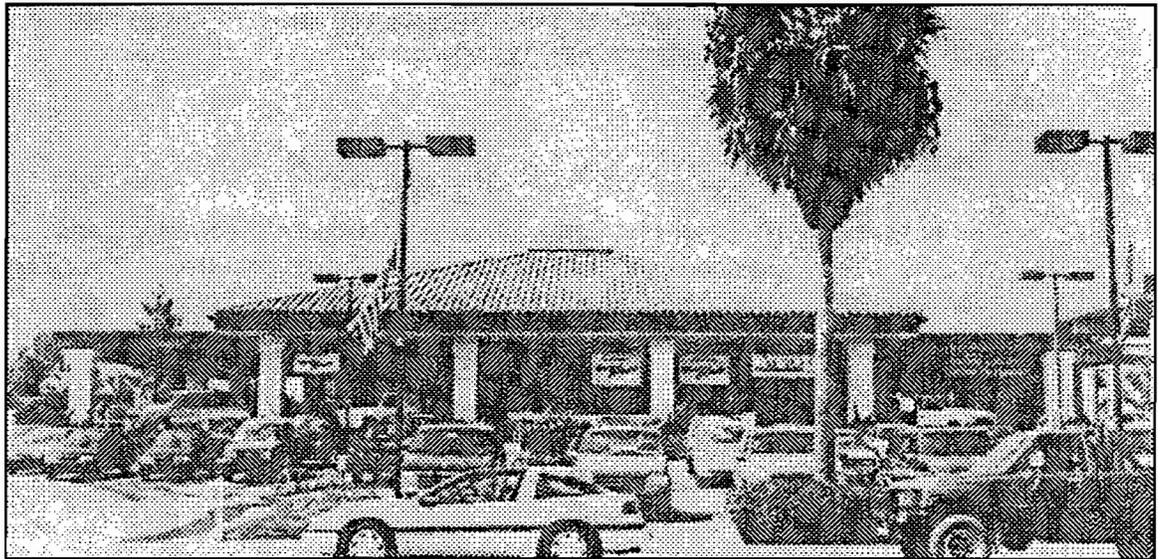
- stucco, smooth, sand, or light lace finish
- wood, as an accent material
- brick, as an accent material
- wrought iron
- tile, as an accent material
- split-face block (storage screening only)

### **Inappropriate**

- metal or aluminum siding
- wood shingle
- log cabin
- clapboard or plywood siding
- unfinished concrete block
- simulated wood, stone, or masonry.

### **Massing, Scale, and Articulation**

The Early California style uses simple, strong columns that reflect the dominant use of masonry or stuccoed masonry. Mass predominates over openings in the percent of each expressed on primary wall surfaces (except for showrooms).



*Appropriate scale and massing for auto dealerships.*

All ground-story facades should be designed to relate to a pedestrian scale. This can be accomplished through breaking the facade into bays and minimizing sign size and location. Arcades and trellises are strong elements that create pedestrian scale.

A variety of massing and seemingly accidental arrangement of building forms imparts a charming and picturesque quality. Buildings should have a sense of having been added onto over a long period of time. Large buildings should be broken up into smaller segments, giving them an asymmetrical character.

### **Appropriate**

- articulation of wall planes
- projections and recesses to provide shadow and depth
- simple, bold forms
- roof overhangs
- appearance of thick walls

**Inappropriate**

- unarticulated, vast expanses of wall surface
- box-like buildings without horizontal and vertical wall articulation
- steeply pitched roof or flat roofs

**Color**

Color is a primary theme-conveying element. In general, walls should be predominantly off-white, cream, or light pastels while avoiding strong pink and salmon hues, ochre, and stark white.

Wood should be light brown stain or kept natural for a weathered look.

Accent colors may be used to impart a festive quality to buildings. Accent colors are frequently brilliant shades of blue, ochre, red, deep blue-green or yellow and are typically used around doors and windows.

**3. Balconies**

Incorporation of balconies onto or within building forms is encouraged for both practical and aesthetic value. Balconies should be used to break up large wall masses, offset floor setbacks, and add human scale to buildings.

**Appropriate**

- smooth, sand, or light lace finish stucco
- simple, clean, bold projections
- balconies with articulated wall surfaces
- ceramic tile accent
- railings of wood or stucco half wall

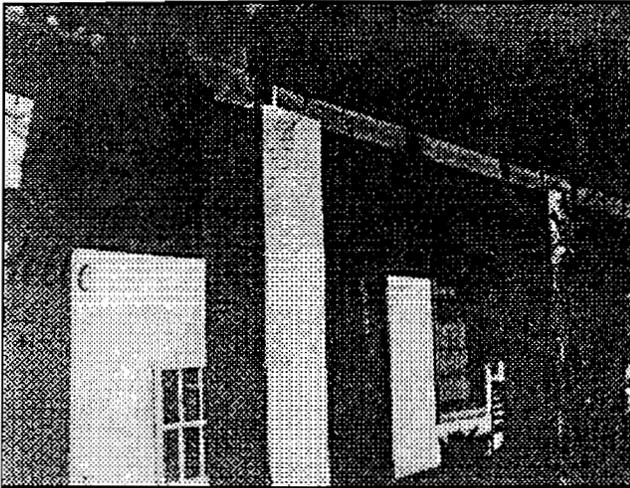
**Inappropriate**

- transparent walls, such as glass or plastic

#### 4. Columns and Archways

Stucco columns should be square, rectangular, or round, and massive in thickness. Capitals and column bands are encouraged. A base should be incorporated at the bottom of all columns. Column height should be 4-5 times the width.

Arches should be semi-circular or slightly flat and in regular series with columns as supports or walls. Square-cut arcade openings are allowed but should be kept to a minimum, and parabolic arches should never be used.



*Flat arches are appropriate.*



*Appropriate arch and column combination.*

## 5. Doors

Doors in this style are typically rectangular or round-headed, are made of wood, are deeply set into walls, and have a "heavy" appearance. The doorway can be arched with a glass fan window above the door header.

Trim may be tile or painted. The doors themselves are usually painted or stained to contrast with the building color. Where metal doors are used, they must be anodized dark bronze.

### Appropriate

- double wood doors
- single wood doors
- multi-lite doors

### Inappropriate

- barn doors
- Dutch doors
- non-anodized aluminum glass doors (anywhere)
- non-anodized aluminum screen doors



*Appropriate door detail.*

## 6. Exterior Stairs

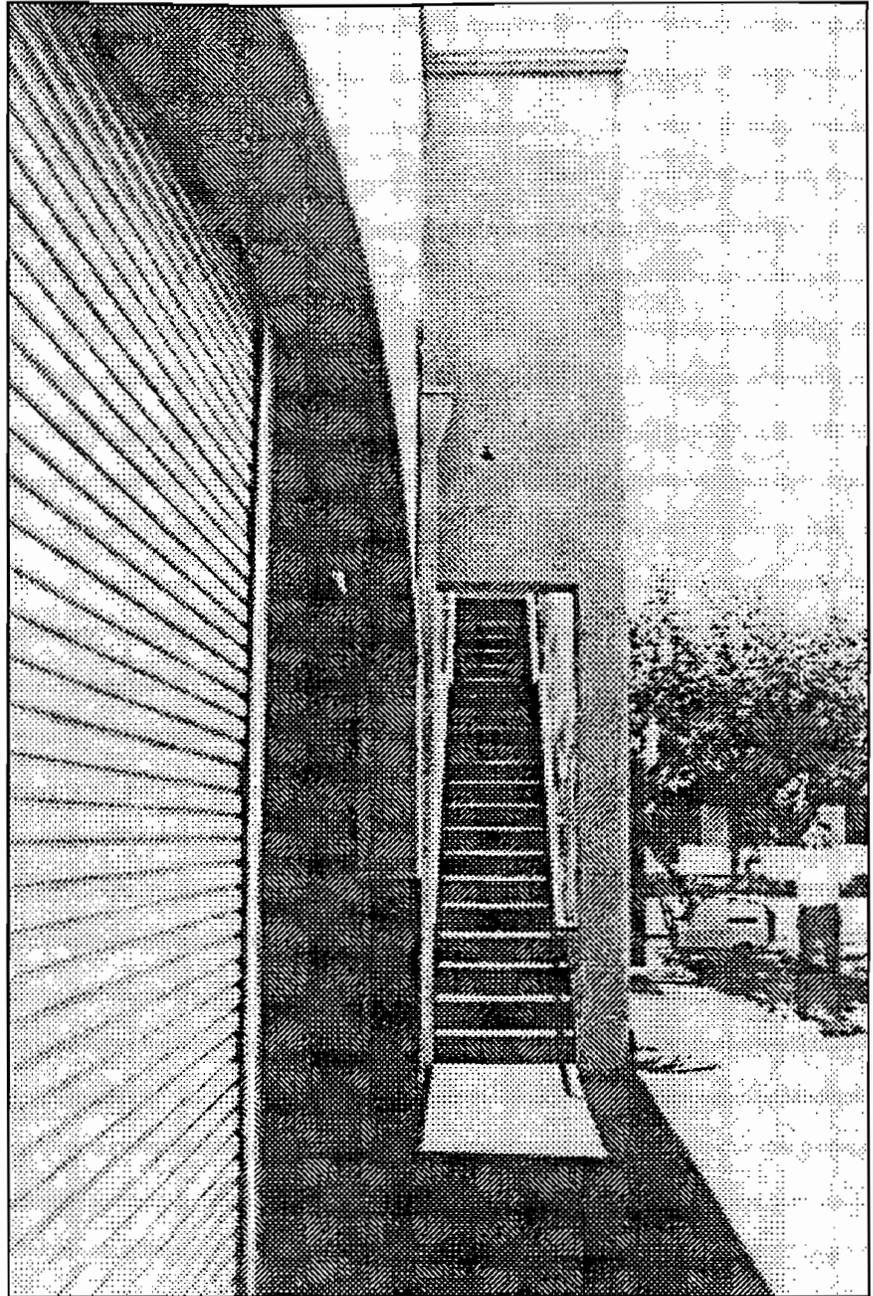
Simple, clean, bold projections of stairways are encouraged to complement the architectural massing and form of buildings. Stairways should be of smooth stucco or plaster with accent trim of complementary colors. Tile is sometimes incorporated for accent.

### Appropriate

- side walls of smooth or sand finish stucco
- accent trim cap or banding of tile

### Inappropriate

- exposed prefabricated metal stairs
- open metal railings



*Minimize exposed exterior stairwells.*

## 7. Gutters, Downspouts, and Vents

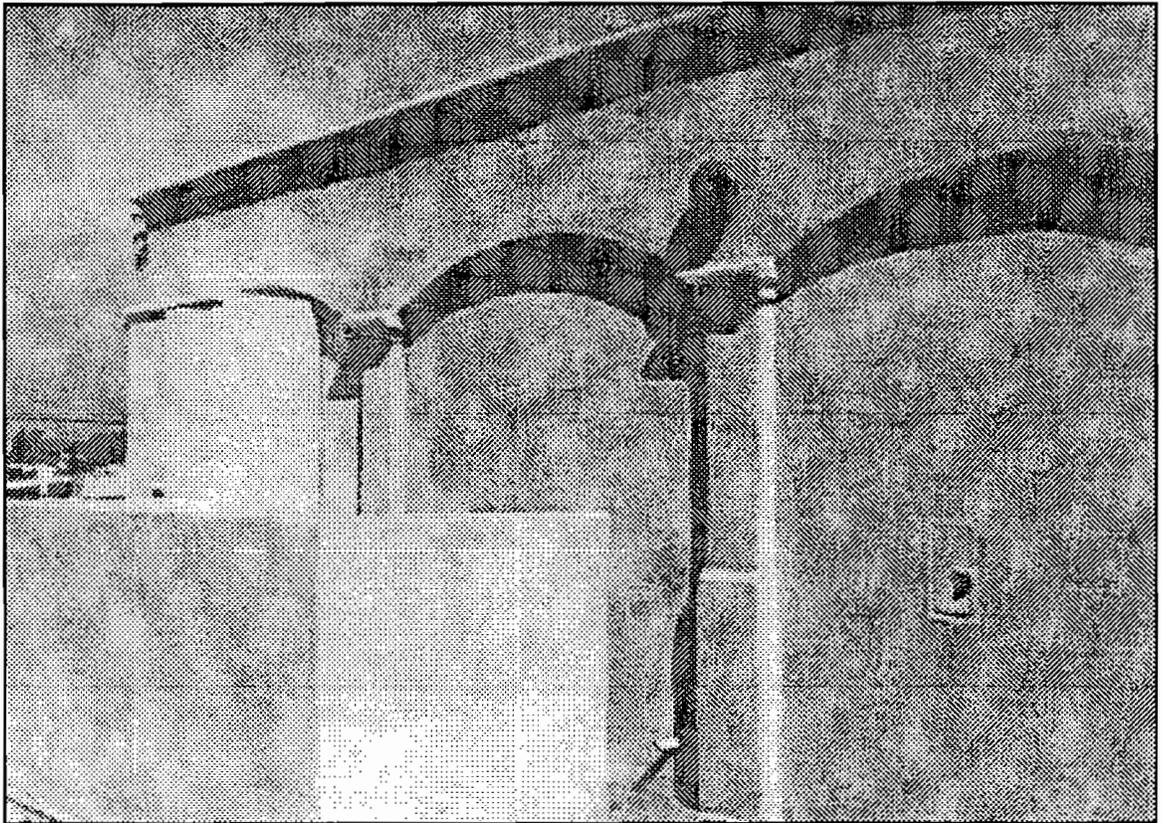
Gutters and downspouts should be placed away from street or freeway view whenever possible, unless designed as a continuous architectural feature. Exposed gutters used as an architectural feature should be colored to match fascia or wall material. Exposed downspouts should be colored to match the surface to which they are attached. Where possible, downspouts should be placed within columns.

## 8. Paving

Textures, patterns, and colors are encouraged in the design of paved areas. This type of enhancement is particularly helpful when used to separate and highlight outdoor display areas, parking, and vehicle storage and when used with landscaping. Texture should define direction of walkways and location of doors. Large monolithic areas of single-color untextured paving are discouraged.

## 9. Rear and Side Elevations

Each wall visible from a street, parking lot, or adjacent property should be treated as a major facade and should be designed for public view. These areas should also be appropriately landscaped in accordance with the standards of this specific plan and the provisions of the Downey Municipal Code.



*Architectural detailing on blank walls breaks monotony.*

## 10. Roofs/Mechanical Equipment/Solar Panels

### Roofs

Due to the visibility of roofs within the specific plan area from two freeways, the roof pitch for the Early California style should be low. A 4:12-6:12 roof pitch is appropriate for all main roof surfaces. Roof pitch for attached arcades or colonnades should be minimum 2:12. Roof types should be limited to low-pitched gable roofs, with occasional use of a hip roof or shed roof as an accent at the end of a building. Roofs may be comprised of the standard barrel tiles, "S" tiles, or "U" tiles. A random application of roof tile is to be achieved. Roof tile color should vary, but blue or orange tiles will not be allowed.

### Mechanical Equipment

Rooftop equipment that is built into the architecture of the building is preferred. This can be accomplished through the use vented gables or hipped roof elements, equipment wells hidden by the low-pitched Early California roofs, or similar methods. Rooftop equipment may be incorporated into a sloping roof and screened by a trellis or other structure that is consistent with the building materials. No rooftop equipment wells shall be allowed to face a freeway. Flat roofs that incorporate a mansard roof edge as a screen may be used but are not encouraged and shall not be allowed where the equipment is visible from a freeway or off-site location.

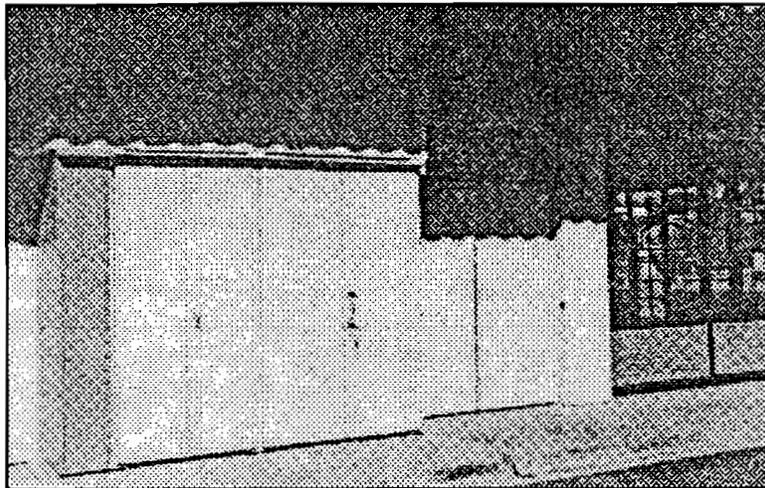


*Screen all rooftop equipment from view.*

All vents, louvers, exposed flashing, tanks, pipes, stacks, sheet metal, overhead doors, rolling and service doors shall be painted consistent with the color scheme of the building.

All equipment, whether on the roof side of a building or on the ground should be screened. Screening should blend with the building design in terms of material, color, shape, and size. Where equipment is separated, a continuous screen is desired. All transformers should be placed underground. Where this cannot be done, transformers must be screened and may not be located in a building setback area. Prefabricated metal utility cabinets are highly encouraged to hide electrical wiring and cables. They must be painted to match adjacent walls.

No television, radio, or other electronic antenna or device of any type shall be erected or permitted to remain on a property without the approval of the City Planner, pursuant to the provisions of Section 9136 of the Downey Municipal Code.



*Enclose utility equipment in cabinets.*

### **Solar Panels**

Use of solar energy is highly encouraged. However, consideration must be given during the design stage to integrate solar panels into the overall building design. Solar panels and any support equipment such as racks must be screened from view of public rights-of-way and painted to match the roof color.

#### **Appropriate**

- gable and hip roofs
- shed roofs
- combining one and two story roof elements
- creating articulation in ridgeline plane
- varying plate heights and ridge heights

#### **Inappropriate**

- large expanses of flat roof

## **11. Screenwalls**

Screenwalls or backdrops are often used to separate and screen outdoor display and customer areas from non-customer areas such as automobile storage and service bays. Such walls should be constructed of materials that are compatible with the building materials and colors and should be kept relatively free of embellishment or decoration. Where screenwalls function as a visual and sound barrier from residential or other adjacent uses, the rear of the wall should be additionally treated with vines or other landscaping that will deter graffiti and enhance the view of the wall from adjacent property. Screenwalls should not be constructed adjacent to buildings or other screenwalls in such a manner as to create narrow caverns or areas that may accumulate trash or otherwise be a safety hazard. Use of the rear walls of service or other on-site buildings to separate customer areas from other functions is encouraged.

All masonry walls should be of an ornamental design in texture and shadow pattern when visible from a street or freeway.

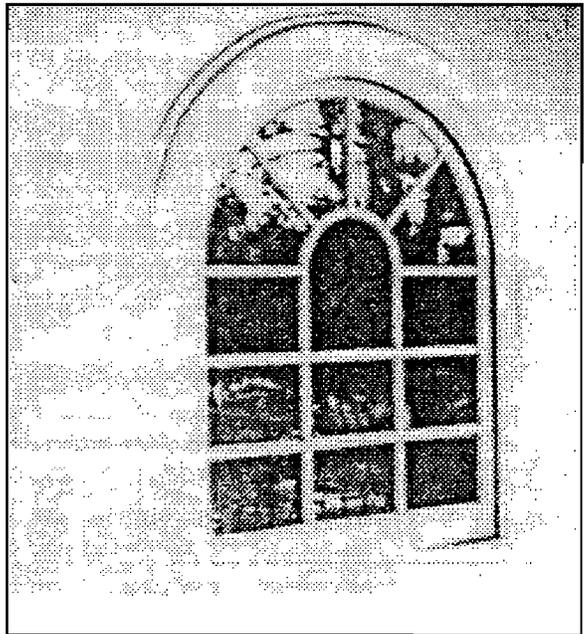
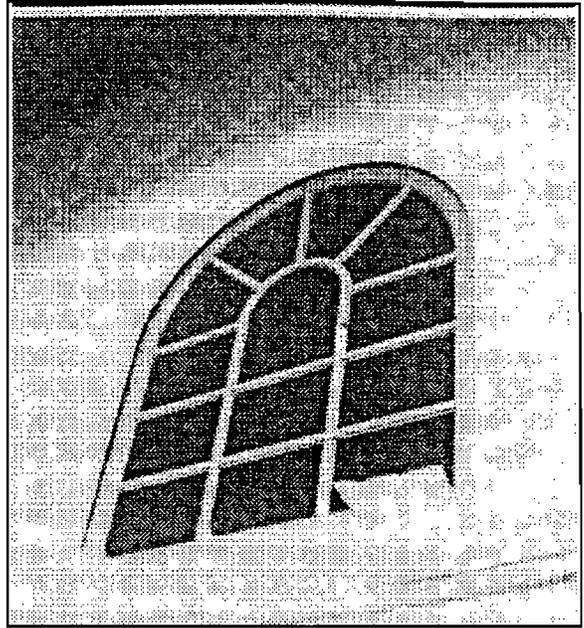
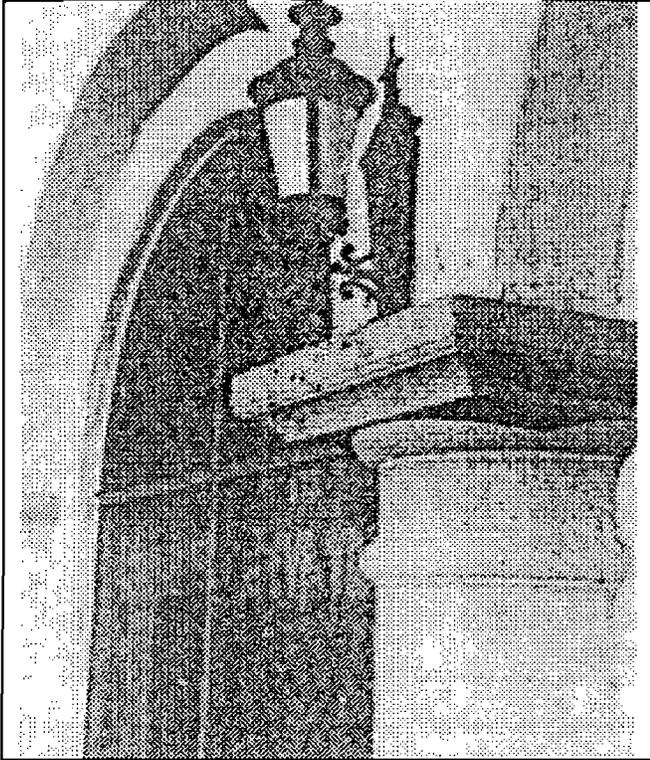
All screenwalls facing a public street should be located to the rear of the front yard setback with landscaping in front.

## **12. Windows**

Windows are typically rectangular but may include round-headed openings with various forms. Multi-lighted French doors are often used as well as bay windows. The appearance of the window being recessed into the wall is an important characteristic. This can be achieved by using wood or metal windows set into a stucco plant-on surround. Where metal-framed windows are used, they should be dark bronze anodized aluminum.

Large showroom display windows should not be designed in a typical square storefront fashion, but should be incorporated into the architectural style through the use of the elements described above.

Wrought iron and wood window grilles are also important characteristics of the style, but their use is normally limited to lower floors for accent or as a security measure. On upper floors, windows may feature shallow balconies with wooden or wrought iron railings or plaster half-walls. Other forms of window ornamentation include tile or plaster surrounds and hooded pediments. Attention must also be given to the shading of windows with southern and western exposures.



**Appropriate**

- bay windows
- French doors
- multi-lite windows
- rectangular windows
- round-headed windows
- clerestory windows
- round windows

**Inappropriate**

- metal or cloth awnings
- silver or gold window frames
- reflective glass
- windows to ground level (without bulkhead)
- windows that appear flush with exterior wall

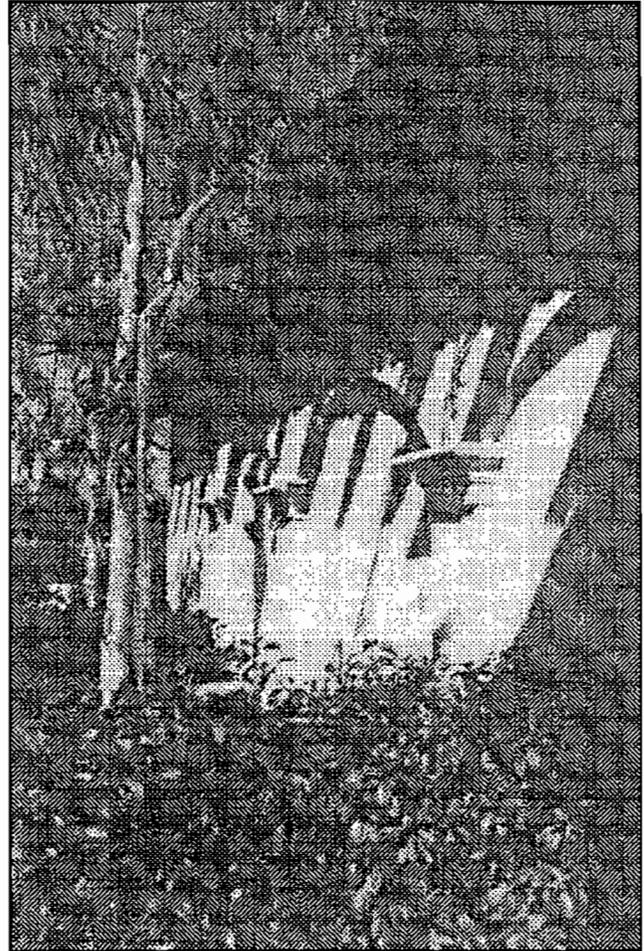
## **E. Landscaping, Lighting and Maintenance**

### **1. Landscaping**

Landscaping must be used in all projects for aesthetic purposes; to soften and enhance facades; to provide shade, wind, and sound protection; and to control dust and erosion. The following required standards and general guidelines describe what type of landscape planning, implementation, and maintenance is expected.

#### **Standards Applicable to All Sites**

- a. Landscaped areas shall be provided adjacent to street property lines:
  - Florence and Studebaker: ten (10) feet
  - Dollison, Fairford, On-Ramps: five (5) feet  
[One (1) foot for Sites 3&7 along Dollison, when the building is set back only 1']
- b. In addition to the landscaped street setback, 2.5 square feet of landscaping shall be provided in the building setback area for each lineal foot of street frontage. For service stations, a total of 15% of the site shall be landscaped.
- c. All landscaping shall have an automatic irrigation system that complies with Section 9152 of the Downey Municipal Code.



### **General Guidelines**

- a. Landscaping and open spaces should be designed as integral parts of any site plan. They should enhance the building design and public views, provide buffers and transitions, provide for a balance of solar uses, and provide screening.
- b. Arbors and trellises are appropriate to the Early California style and may be used to accent the overall design theme.
- c. Landscaped areas should use a three-tiered planting formula that includes:
  - 1) grasses and groundcovers, 2) shrubs, and 3) trees.
- d. The following are common planting design concepts that should be used whenever possible:
  - Specimen trees in informal grouping and rows at major focal points
  - Flowering vines on walls and arbors
  - Use of planting to create shadow and patterns against walls
  - Trees to create canopy and shade, especially in parking areas
  - Berms, plantings, and walls to screen parking or storage areas
  - Shrubbery and creeping vines along walls and fences

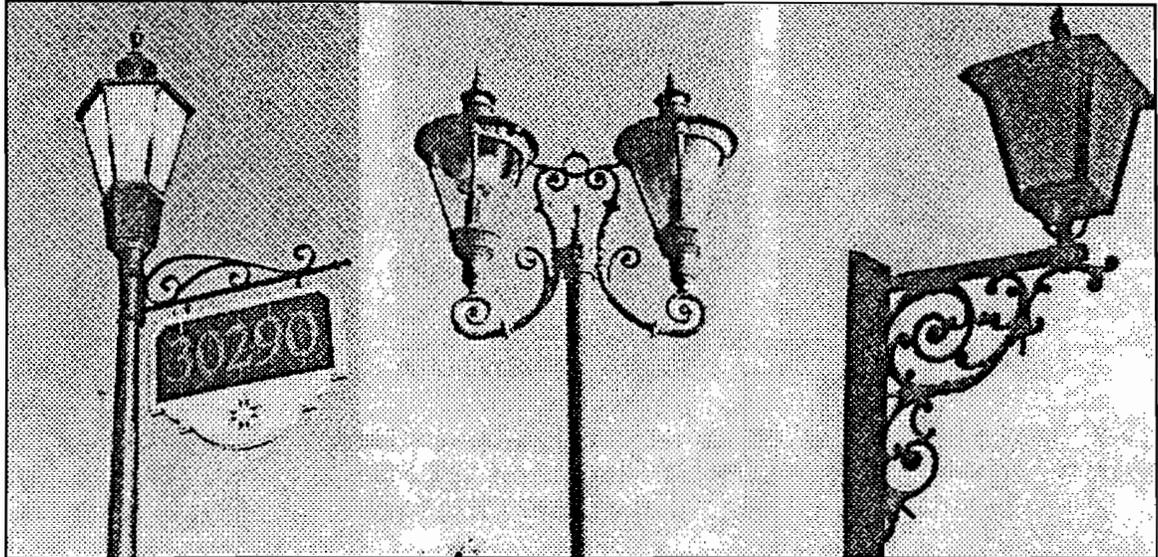
- e. Plant material and planting methods shall be suitable for the soil and climate of the site. Native and drought-tolerant plants should be used whenever possible.
- f. When non-living groundcover (rock, pebbles, white crushed stone, redwood chips, stamped or patterned concrete) is used, it shall be in combination with live plants and shall be limited to an accent feature.
- g. If redwood chips are used, the material must be shredded ("walk-on") bark.
- h. Hedges, low walls, or a combination of hard- and softscape are encouraged to control pedestrian access to outdoor sales areas, or as a security and water conservation measure.

## **2. Landscape Installation and Maintenance**

- a. Trees should be minimum 15 gallon to 36" box size and be at least eight feet tall at time of planting. Larger initial tree sizes are recommended. Herbaceous plants must be minimum one gallon size at time of planting. Shrubs must be a minimum of five gallons. Variation of 5-15 gallon sizes is encouraged. Trees should be adequate in trunk diameter to support the top. Trees, shrubs, and vines should have body and fullness typical of the species.
- b. All groundcover should be healthy, densely foliated, and well-rooted cuttings, or one gallon container plants. Herbaceous and flat plant groundcovers should be planted no more than 18 inches on center. Woody, shrub groundcover should be planted no more than four feet on center.
- c. The spacing of trees and shrubs should be appropriate to the species used. The plant materials should be spaced so that they do not interfere with the adequate lighting of the premises or restrict access to emergency apparatus such as fire hydrants or fire alarm boxes. Proper spacing should also ensure unobstructed access for vehicles and pedestrians.
- d. All landscaped areas and irrigation systems must be properly maintained throughout the life of the project. Maintenance shall consist of regular watering, mowing, pruning, fertilizing, clearing of debris and weeds, removal and replacement of dead plants, and repair and replacement of irrigation systems and integrated architectural features.

### 3. Lighting

Lighting is part of the architectural vocabulary and can help create and dramatize the nighttime image of a structure. Each fixture must also be attractive during the day when the pole base and light add another dimension to the scene. The overall size and scale of fixtures is also important.



#### Standards Applicable to All Sites

- a. Display areas shall be lit by sharp cut-off fixtures directing light downward only. Particular attention should be paid to prevent light spillover to public rights-of-way and residential properties. Lights should be shielded, diffused, or indirect to avoid glare to pedestrians and motorists.
- b. No lighting over five (5) feet in height shall be permitted within twenty (20) feet of the public right-of-way.
- c. Lighting standards closest to public streets shall not exceed twenty (20) feet in height and shall have no more than three fixtures per standard at a minimum of thirty (30) feet on center. Average wattage of display areas adjacent to Florence Avenue and Studebaker Road shall not exceed 1.3 watts per square foot.
- d. The interior of display areas may be lit by fixtures no closer than sixty (60) feet to the line of standards closest to the street. Interior light standards shall be no higher than twenty (20) feet, shall be spaced no closer than sixty (60) feet on center, and shall have no more than four (4) fixtures per pole.
- e. Service, storage, and employee parking areas shall be lighted by standards no higher than twenty (20) feet. Average wattage for the entire storage area shall not exceed 0.3 watts per square foot.

- f. Strings of incandescent fixtures shall not be allowed in any exterior area.

**General Guidelines**

- a. All exterior lighting, with the exception of lighting for public streets, should be complementary with the Early California architectural style. Metal and heavy wrought iron fixtures are preferred throughout all development areas.
- b. All lighting fixtures on a site should be from the same family of fixtures with respect to design, materials, color, and light color. To minimize the total number of freestanding light standards, wall-mounted lights should be used to the greatest extent possible.

## F. Streetscape Plan

A coordinated landscaping and improvement plan can establish an identity for the specific plan area and create visual continuity between different developments and land uses. Developers of any parcel shall be required to install the features of this streetscape plan in parkway adjacent to their development. The plan below will apply to all street frontages. Additional features are required at each of the three major intersections.

### 1. City Entry Sign

A monument sign originally intended to denote the City Limits sits in the median of Florence Avenue immediately east of the I-605 overpass. This is not, however, the actual boundary, and the incorrect placement leads motorists to believe that the specific plan area is not part of Downey.

A new City entry sign, compatible with the City's entry sign program as it may be developed, should be placed in the median farther east on Florence Avenue—perhaps just west of Fairford (the median east of this point is not wide enough). To mark the true boundary of the City, a smaller entry sign could be placed at the side of the road on the I-5 overpass, or on Sites 1 or 5. These sign would be a metal pole sign similar to standard "No Parking" signs, or it could conform to the City's entry sign program. Entry signs would be installed at the time the raised median was installed on Florence between Studebaker and Fairford.

### 2. Sidewalk Plan

- a. The parkway between a developed parcel and any paved roadway shall be planted as follows:
  - Mexican Fan Palms (*washingtonia robusta*) shall be planted in 4' square grates approximately 50' on center. If the existing parkway is completely paved, the grates and trees shall be added.
- b. That portion of a parcel adjacent to the parkway shall be developed as follows:
  - Queen Palms (*arecastrum romanzoffianum*) shall alternate with those in the parkway, and shall also be spaced approximately 50' on center. They shall be located in the planting setback area described in Section III of this specific plan.

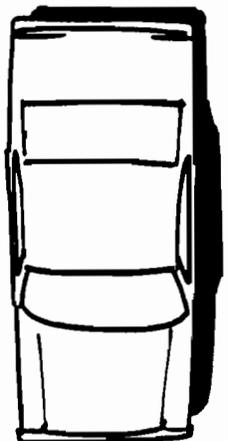
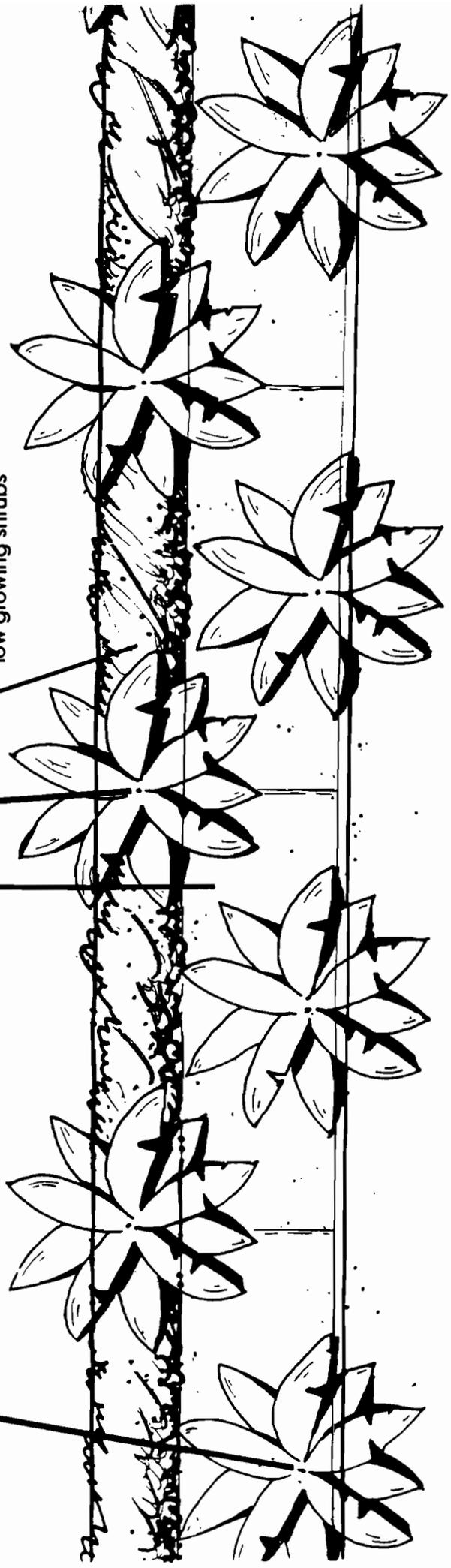
# TYPICAL SIDEWALK CONCEPTUAL DESIGN

Existing concrete sidewalk  
Natural colored broom finish

Mexican Fan Palm trees approx. 50' o.c.  
in 4' square tree grates.

Queen Palm trees on private property  
at approx. 50' o.c. alternating with  
the sidewalk street trees.

5' Planting strip with  
low growing shrubs

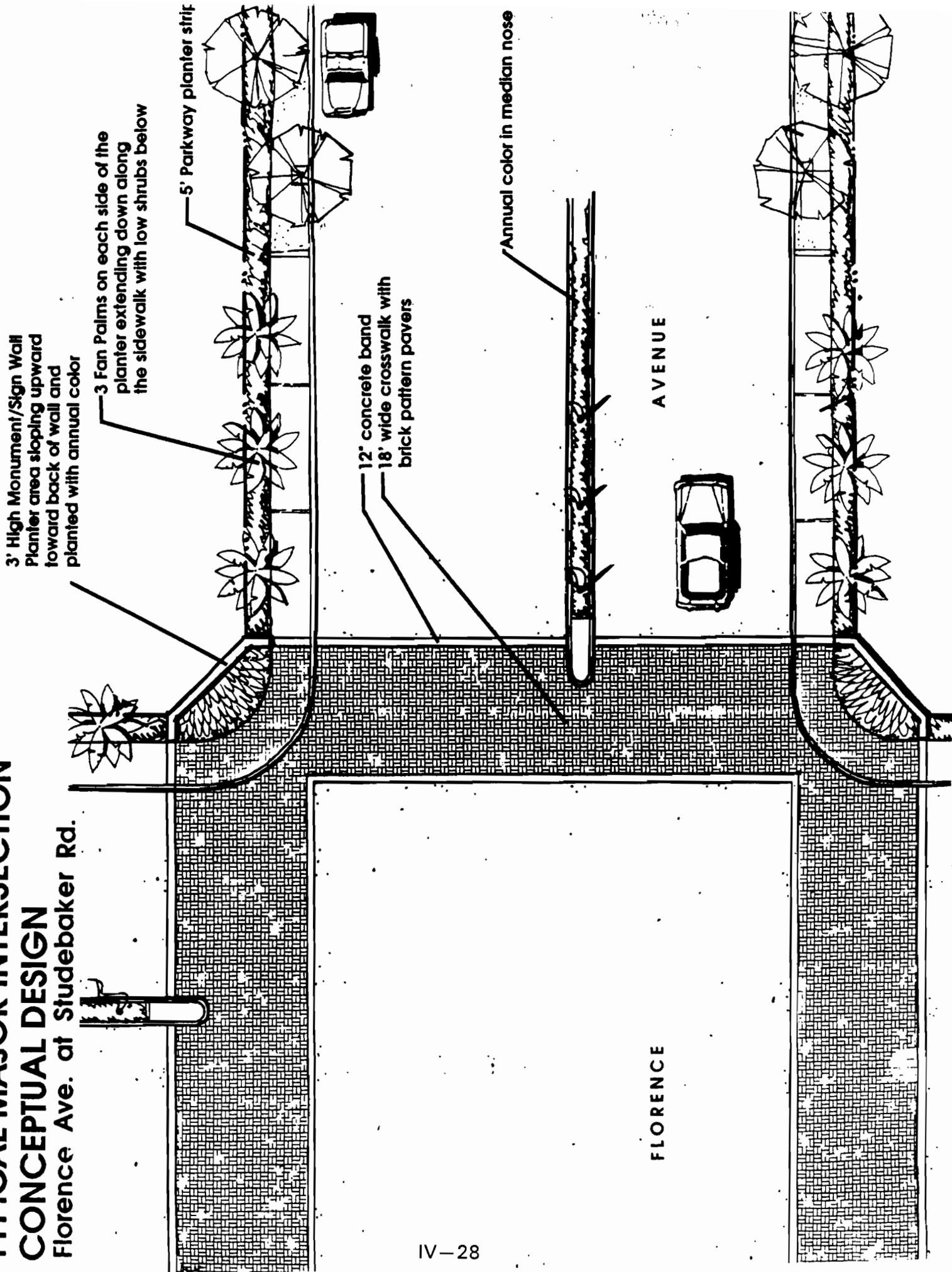


### **3. Florence/Studebaker Intersection**

- a. **Parcels abutting the intersection of Florence and Studebaker, in addition to the requirements of the typical sidewalk plan, shall have a triangular planting area extending from the corner approximately 10 feet along each street. This planter shall be framed by a 3' high wall (which may be used for a sign). The planting shall slope upward from the intersection toward the wall and shall be planted with annual color.**
- b. **The Florence/Studebaker intersection shall additionally be improved as follows:**
  - **Brick pattern pavers shall be used to identify the four crosswalks at the intersection.**
  - **Raised planted medians shall be installed on Florence and Studebaker. Mexican Fan Palms shall be used in these planters to be consistent with the sidewalk plan. The median noses shall be planted with annual color.**

# TYPICAL MAJOR INTERSECTION CONCEPTUAL DESIGN

Florence Ave. at Studebaker Rd.



#### **4. Studebaker/Freeway Ramp Intersection**

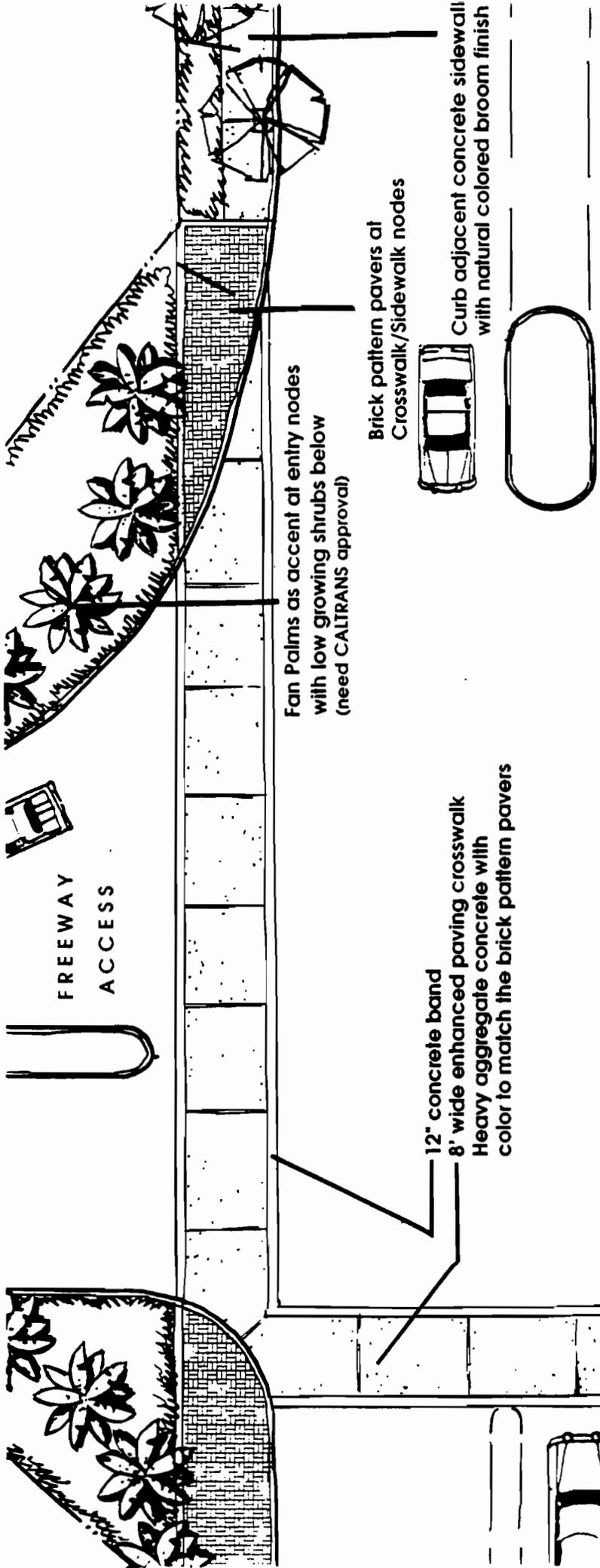
The parkway adjacent to parcels at the intersection of Studebaker and the freeway on/off ramps, in addition to the requirements of the typical sidewalk plan, shall have the following:

- Brick pattern pavers used as a sidewalk material at the intersection.
- Crosswalks shall be 8' wide and shall be enhanced with heavy aggregate concrete paving, colored to match the brick pattern pavers of the sidewalk. The Crosswalks shall be bordered by a 12" decorative concrete band on each side.
- A 48" masonry wall shall replace the chain link fence at the closed entrance to Dollison Drive. An Entry sign may be placed at this location.
- Fan palms as used in the sidewalk plan shall mark the closed entrance to Dollison Drive.

#### **5. Florence/Fairford/Freeway Ramp Intersection**

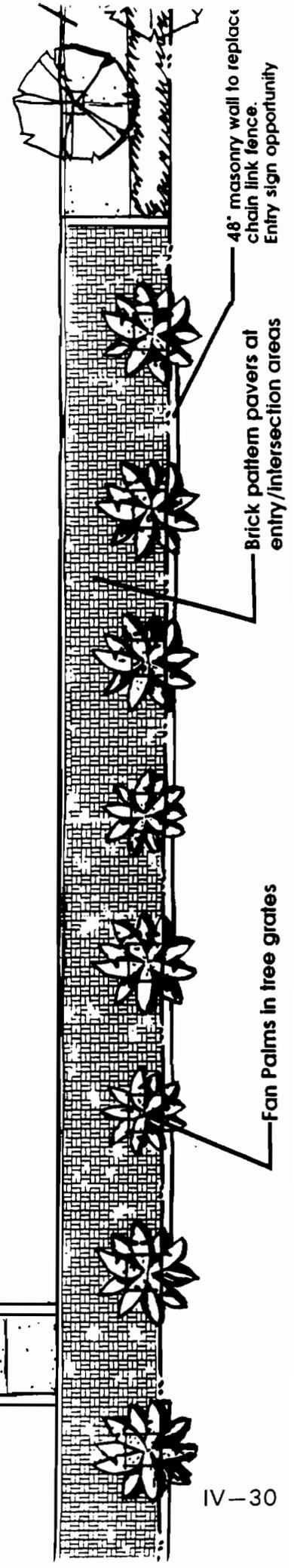
The parkway adjacent to parcels at the intersection of Florence and the I-5 on-ramp, in addition to the requirements of the typical sidewalk plan, shall have the following:

- Brick pattern pavers used as a sidewalk material at the intersection, extending 80 feet from the corner.
- Crosswalks shall be 8' wide and shall be enhanced with heavy aggregate concrete paving, colored to match the brick pattern pavers of the sidewalk. The Crosswalks shall be bordered by a 12" decorative concrete band on each side.
- The ends of each median shall be planted with annual color.



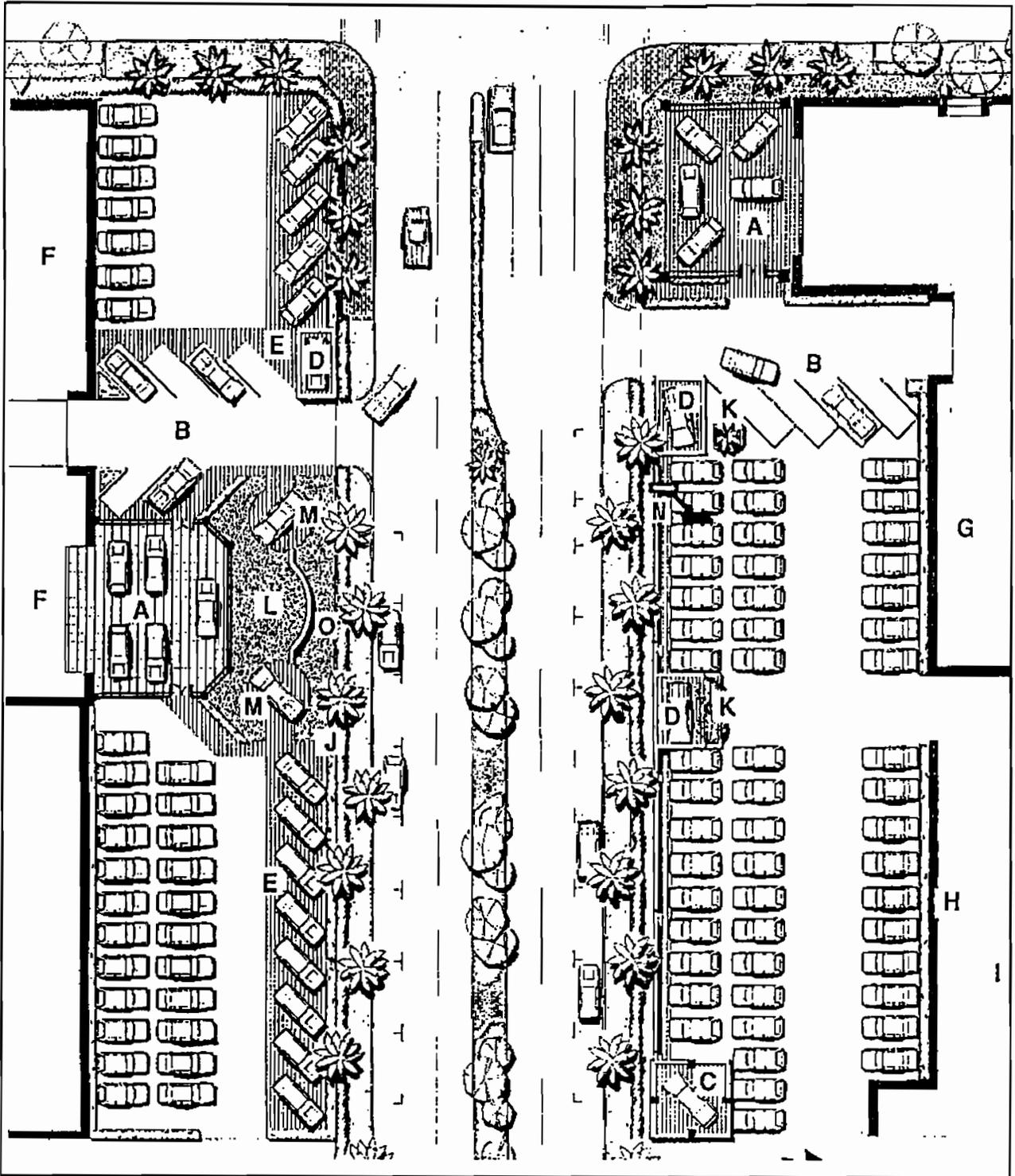
## TYPICAL MINOR INTERSECTION CONCEPTUAL DESIGN

Studebaker Rd. at Freeway Access  
Florence Ave. at Fairford Ave.



## G. Dealership Prototypes

The figure below illustrates many of the design features discussed in this section.



The dealership prototype on the right side of the figure includes a showroom at the corner of the property and a covered outdoor display pavilion at the opposite corner of the site for balance. Between these features are the new and used car display areas which feature two podiums and a landscaped security barrier. The left side of the figure shows a prototype with a showroom in the middle of the property. It is set back slightly from the street and is highlighted by outdoor display pads located in a landscaped area with a monument sign. Display areas are located to the left and right of the showroom and feature special paving along the front to highlight the cars and make a memorable impression.

### **Showrooms**

Both showrooms (A) are "open" with large unobstructed windows on three sides. Their size and placement make them the prominent architectural features of their sites. Customer parking areas (B) are adjacent to the showrooms and clearly defined.

### **Outdoor Sales Areas**

Both prototypes feature an orderly arrangement of displayed vehicles with generous spacing between the cars. Special features in these areas include a display pavilion (C), several display podiums (D), and special paving to highlight the automobiles (E).

### **Backdrop**

Both sites feature solid backdrops (with landscaping along the edges) to enhance the appearance of the displayed automobiles. Service and storage areas are well out of sight of the sales areas in both plans. The left prototype uses the front wall of the service buildings (F) as a backdrop for the sales areas. The right prototype uses the service department wall (G) for a portion of the backdrop as well as a freestanding wall (H) to visually separate the outdoor sales area from the dealer's storage and on-site parking areas (I). Service entrance doors/gates should be limited to one opening per street frontage. Individual service bays may not be visible from Florence or Studebaker.

### **Landscaping**

Both plans show how generous landscaping can effectively highlight the showrooms, outdoor display areas, and the dealership as a whole. Both plans feature low landscaped security barriers (J) which enhance the appearance of the sales area and prevent autos from being driven out the front. The plan on the right shows how several carefully placed small trees (K) can divide the large display area into smaller, more comfortable areas. The left plan features a spacious "lawn" (L) with concrete pads (M) for display of specially featured cars.

## **SECTION V – CIRCULATION PLAN AND REQUIREMENTS**

The circulation plan provides the transportation system and basic standards for safe and efficient vehicular and pedestrian movement within the specific plan area. It sets forth the implementation of improvements to the circulation network, construction of off-site intersection and roadway improvements. Streetscape design standards are described in Section VII of this document. As part of the EIR for SP90-1, a traffic study was prepared that evaluated the impacts of the high-intensity commercial development anticipated. The mitigation measures required are included in this Specific Plan.

### **A. Classification of Streets**

The following streets comprise the roadway network within the Specific Plan area:

Arterial Streets	Florence Avenue (east-west) Studebaker Road (north-south)
Local/Collector Streets	Dollison Drive (east-west) Fairford Avenue (north-south)
Freeways	Santa Ana Freeway (I-5) San Gabriel River Freeway (I-605)

### **B. Improvements**

Implementation of this specific plan will result in the generation of up to 4,150 average daily vehicle trips. The traffic study completed for this specific plan analyzed the current levels of service at each intersection and distributed additional trips generated by specific plan development over the road network. Project-related traffic will increase volumes on the roadway systems from less than one percent to approximately 13%.

Without mitigation, the proposed project will significantly affect three of the six intersection studied in the traffic report. These three intersections are:

- Florence/I-605 southbound ramp (west of the study area)
- Florence/Studebaker
- Florence/Fairford

The following mitigation measures will be required to prevent significant negative effects on the traffic system:

1. New developments shall prepare a Transportation Demand Management (TDM) program, where feasible, to reduce traffic generated during peak periods. This could include staggered work schedules, ridesharing programs, priority parking for car-and vanpools, transit service enhancements, and bike storage lockers.
2. New developments shall conduct a traffic analysis to determine whether the project in question will increase traffic to such levels that the street widenings re-stripings or other physical improvements noted below should be completed concurrent with the project.

3. Florence Avenue shall be constructed through the Specific Plan Area as a six-lane arterial with a landscaped median and left-turn lanes at all intersections, except at the intersection of Florence and the I-5 southbound on-ramp.
4. Provide an additional westbound-to-southbound left-turn lane at Florence/I-605 Southbound Ramp.
5. Reconstruct the Florence/Studebaker intersection so that Florence will have three through lanes in each direction, an exclusive right-turn lane for the eastbound-to-southbound movement, and double left-turn lanes for the westbound-to-southbound movement.
6. Improve the Florence/Fairford intersection to provide three through lanes in each direction on Florence with a westbound-to-southbound left-turn lane.
7. Synchronize the signals along Florence to enhance efficiency and improve traffic flow.
8. Landscaping and signs shall be designed and installed to ensure good visibility for motorists at all access driveways and roadway intersections.
9. Construct sidewalks along all streets and pedestrian walkways between all buildings and parking areas.
10. Provide bus shelters where projects front Florence and Studebaker, subject to approval by RTD.
11. Ingress/egress to Site 2 shall be limited to Fairford Drive to avoid conflict with Florence Avenue traffic.
12. Vehicular access from Site 3 and Site 7 to Dollison Drive shall be prohibited to discourage additional traffic in the residential neighborhoods to the south.

### **C. Freeway Ramp and Surplus Caltrans Property**

Site 5 is an area of approximately 3.5 acres that is currently owned by Caltrans. It is used as a southbound I-5 on-ramp for westbound vehicles on Florence. This specific plan includes development standards for Site 5, in the event that Caltrans decides to declare it surplus. At this time, Caltrans has no plans to dispose of the land, which may be used as part of a proposed project to widen I-5.

If the parcel were vacated and sold, the freeway on-ramp would be closed. At this time, eastbound vehicles on Florence cannot use this ramp. No left turns from Florence are allowed. In evaluation for the EIR, the traffic consultant concluded that it is not advisable, based on existing and projected traffic patterns, to accommodate left-turns from eastbound Florence to the on-ramp. Traffic that would make this movement can use the existing on-ramps at Studebaker Road, and avoid causing additional congestion on Florence Avenue. If the ramp were closed, however, westbound vehicles on Florence and northbound vehicles on Fairford that now use the ramp would also be required to use the

Studebaker on-ramps. This may increase traffic at the Studebaker/Florence intersection, although the improved left turn pockets from Florence to Studebaker should accommodate this.

#### D. Fairford Realignment

At the present time, that portion of Fairford Avenue north of Dollison Drive is not aligned with the portion south of Dollison Drive. When Sites 2 and 3 redevelop, Fairford should be realigned. The opening at Florence Avenue would remain in place, while the opening at Dollison would be moved to the west. This is a minor adjustment that would not have a significant impact on traffic on either Dollison or Fairford.

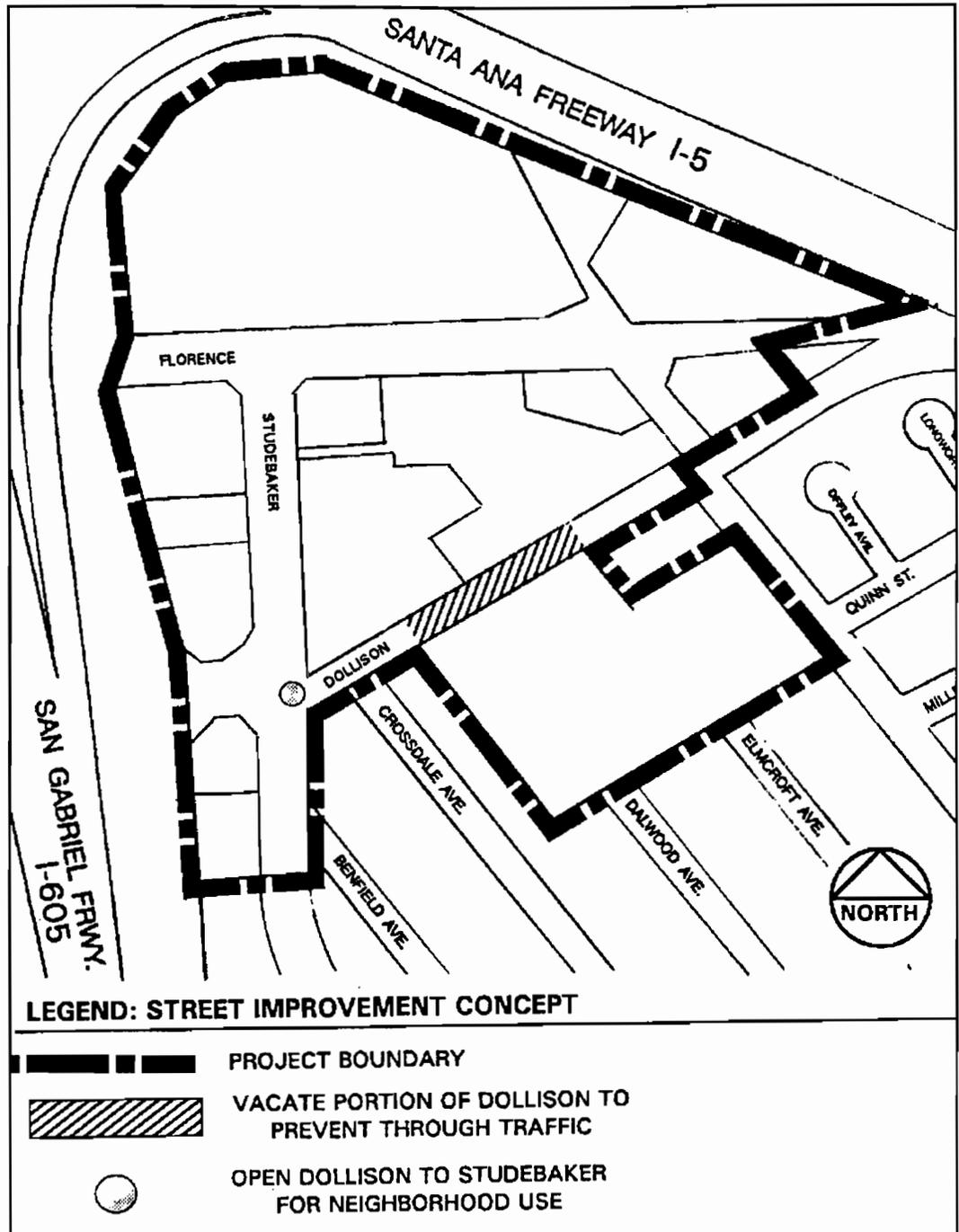
#### E. Controlling Test-Drive Traffic

In addition to employee and customer traffic, auto dealers generate vehicle trips through test drives of cars. Such trips frequently occur on residential streets near the dealership. Test drives from new and existing facilities should avoid the residential streets to the south of the specific plan area. The following measures are proposed to discourage test drive traffic on Fairford, Crossdale, Cecilia, and other nearby streets.

1. Install signs that read  at the following locations:

- Benfield southbound at Studebaker
  - Cecilia eastbound at Studebaker
  - Cecilia westbound at Studebaker
  - Crossdale northbound at Cecilia
  - Crossdale southbound at Dollison
  - Dollison eastbound at Fairford
  - Dollison northbound at Cecilia
  - Fairford southbound at Dollison
  - Fairford northbound at Cecilia
  - Little Lake southbound at Wilderness Park
2. New dealerships will be required to submit plans of typical test drive routes to the City Traffic Engineer for approval. At least four routes should be approved for each dealership, so that test drive traffic is dispersed throughout the area.
  3. A "No Right Turn" sign shall be installed on Dollison eastbound at its intersection with Fairford.
  4. Dollison should be vacated between Sites 6 and 7, blocking the street to through traffic. Vehicles leaving Site 6 would travel through Sites 7 and 3 and exit on Florence Avenue. Once this vacation occurs, Dollison should be opened at the signalized intersection with Studebaker. This would restore ingress/egress to the Crossdale neighborhood that would be lost by Dollison's closure. This proposal is shown in the exhibit on the following page. A "No Outlet" sign should be installed at Dollison/Studebaker to discourage vehicles exiting the freeway from entering the neighborhood.

This plan must be reviewed and conditioned by the Downey Fire Department to ensure that emergency access to Crossdale and Site 6 would not be hampered. Section 7100 of the Downey Municipal Code required public hearings for both the street vacation and opening. Detailed review of impacts of these actions and additional opportunity for neighborhood residents to speak on this proposal will occur at that time.



*Dollison Closure Concept*

5. Vehicles shall be prohibited from traveling directly across the Florence/Fairford intersection from Site 5, if that parcel is developed.

## **SECTION VI – WATER DISTRIBUTION SYSTEM**

### **A. Domestic Water Supply and Fire Flow**

Water supply and fire flow within the specific plan area is provided by three agencies: The City of Downey Water Department (fire flow for Site 1 only), Southern California Water Company (domestic water and fire flow for Sites 8-12), and Santa Fe Springs Water Department (all other sites and domestic flow for Site 1).

The City of Santa Fe Springs water system provides water for most of the project area, including all properties east of Studebaker plus those north of Florence (with the exception of fire flow for Site 1, which is provided by the City of Downey). According to the City of Santa Fe Springs, current water sources and supplies are considered adequate to meet the needs of the project area, with the exception of fire flow requirements.

The preferred plan presented in this document would allow conversion of existing uses from primarily retail to an auto-oriented center. Anticipated water consumption at build-out of the specific plan area would actually be less than current requirements. The EIR for this project estimated that current water needs are approximately 21,000 gallons daily. Anticipated future water needs under this specific plan would be 16,000 gallons daily, primarily because Auto Dealers typically use less water than retail developments covering the same area.

The EIR concluded that it is unclear whether existing fire flow would be adequate for the anticipated development, however. Depending upon the characteristics of specific future projects, it may be necessary to upgrade or extend water mains, such as the 12 inch main owned by Downey that now serves Site 1. It may also be necessary to negotiate jurisdictional changes to ensure that adequate water for domestic and fire-fighting purposes is available.

The EIR recommended the following mitigation measures and water distribution requirements.

1. Required domestic water connections within the specific plan area shall be constructed to meet the standards of the City Engineer, City of Downey Water System and/or City of Santa Fe Springs Water System as appropriate, with easements granted to the servicing agency as required to maintain on-site water facilities. Adequate plans for on-site water facilities to serve future development projects within the specific plan area shall be prepared by a registered professional engineer and shall be subject to the approval of the City of Downey and/or the City of Santa Fe Springs. It shall be the principal responsibility of the developer to extend and/or construct new facilities required to serve future development in the project area.
2. Because of isolation of the specific plan area from the Downey City Water System and since the City of Downey is not aware of any plans by the City of Santa Fe Springs to upgrade its system, the City of Downey shall undertake a study to determine the specific needs of future development in the proposed redevelopment area. This study shall be coordinated with the City of Santa Fe Springs and Southern California Water Company to determine whether the current system for supplying

water to the project area should be continued or whether the City of Downey should acquire the existing system and become the water purveyor for all domestic water as well as fire flow requirements within the specific plan area.

3. Installation of water-saving devices shall be included in the design and construction of future projects within the specific plan area. These shall include, at minimum:
  - Low-flush toilets
  - Low-flow showers and faucets
  - Insulation of hot water lines in water-recirculating systems.
4. Automatic irrigation systems shall be installed which restrict landscape watering to early morning and evening hours, in order to reduce evaporation.
5. Drought-resistant vegetation shall be used for landscaping to reduce water consumption.

## **B. Site Drainage**

Adequate site drainage shall be ensured by the following measures.

1. Participation in the appropriate master plan of drainage. Participation shall be determined by the City Engineer and may include construction of master plan facilities, dedication of right-of-way and/or payment of drainage fees.
2. Developers within the specific plan area shall comply with all measures determined to be necessary by the City of Downey and other governing agencies at the time of site plan review.

## **C. Storm Drain Facilities**

Development allowed under this specific plan is not expected to increase the need for storm drain facilities. The existing system, however, is inadequate to accommodate runoff from a 100-year storm. Modifications to some local storm drain systems will be required. The EIR for the specific plan lists the following mitigation measures.

1. Construction of storm drain facilities in conformance with applicable City of Downey codes and standards. An engineering hydrology study will be required to determine the quantity of storm water runoff from sites within the specific plan area. A drainage plan will be prepared by the developer for each site which will identify the on-site and other localized storm drain improvements required to direct storm flows to local storm drains and regional flood control systems. The necessary improvements shall be constructed prior to issuance of a Certificate of Occupancy.
2. Required improvements to local storm drains may also be constructed as needed by the Community Development Commission of the City of Downey. Funding arrangements, including issuance of bonds or other financial instruments, developer reimbursements and assessment and Community Facilities District may also be used.

3. Necessary easements required to maintain the storm drains in Florence, Longworth, Dollison, and Fairford Avenues shall be retained by the City of Downey Department of Public Works.

#### **D. Sewer Capacity**

Development allowed by this specific plan would produce less wastewater than is generated by the existing development, according to analysis conducted for the Environmental Impact Report. This reduction would occur because of the conversion of Site 1 from General Commercial to automotive use, which generates less wastewater per acre. The current sewer system is considered adequate, and significant improvements are not envisioned to be necessary. Mitigation measures to ensure continued adequate sewer capacity are noted in the EIR.

## **SECTION VII – GENERAL REGULATIONS**

### **A. Authority**

The Florence Avenue/I-5 Specific Plan 90-1 is established through the authority granted to the City of Downey by the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457 (Specific Plans).

### **B. Zoning Ordinance**

Upon adoption of the Florence Avenue/I-5 Specific Plan 90-1, all land use designations, development regulations and standards set forth in this plan, to the extent described herein, replace those contained in the applicable sections of the Downey Municipal Code. Terms used in this specific plan shall have the same definitions as given in the Downey Municipal Code unless otherwise defined herein.

### **C. Relationship to Other Regulations**

This specific plan provides policies, standards, and regulations to guide development of the Florence Avenue/I-5 Specific Plan Area. Areas not addressed are governed by the Downey Municipal Code. No provision of this plan is intended to repeal, abrogate, annul, impair, or interfere with any existing ordinance, resolution, or policy, except as specifically repealed by the adoption of this specific plan.

### **D. Conflicts with Other Regulations**

When provisions of this specific plan impose more restrictive regulations than required by any other ordinance, resolution, or policy, the provisions of this specific plan shall govern. This specific plan is regulatory in nature and serves as zoning law for the properties and planning areas involved. Development plans, design review, tract and parcel maps, and other development entitlements shall be consistent with this specific plan, the City of Downey General Plan, and the Downey Municipal Code.

### **E. Amendment**

1. This specific plan may be amended using the procedure by which it was originally adopted and in compliance with the provisions of Section 9170 of the Downey Municipal Code relating to zone changes and Sections 65450-65437 of the California Government Code. All sections or portions of the specific plan to be changed or that may be affected by the change must be included in the amendment.
2. A concurrent amendment to the City General Plan would not be required unless the Planning Commission determines that substantive changes would influence the goals, objectives, policies, or programs of the General Plan. A supplement to the project Environmental Impact Report (EIR) for SP90-1 may also be required if changes significantly affect the intensification of use to the plan area or vicinity beyond the development proposed in the initial analysis.

## **F. Enforcement**

1. The City of Downey Community Development Director or designee shall administer and enforce the provisions of Specific Plan 90-1.
2. Certain provisions in the plan may be administratively changed, subject to the review and approval of the Community Development Director. This decision may be subsequently appealed to the City Council within fifteen (15) days. Such changes may include the addition of information to the text or accompanying maps provided that no significant changes are found to occur and that the development concept and regulations remain intact.

## **G. Environmental Pollution Control/Performance Standards**

The following performance standards shall apply, in addition to all other applicable standards and the mitigation measures set forth in the EIR. All structures and uses shall be subject to periodic City review for compliance with these standards.

### **1. Emissions/Dust/Other Air Pollutants**

No use may be conducted that creates large quantities of dust, toxic emissions, or other air pollutants.

### **2. Mechanical Equipment**

All ground-mounted mechanical equipment, including heating and air conditioning units and trash receptacle areas shall be completely screened from surrounding properties by use of a wall or fence or shall be enclosed within a building.

### **3. Electrical Disturbance, Heat, Cold, and Glare**

No use except a temporary construction operation shall be permitted which creates changes in temperature or direct glare, detectable by the human senses without the aid of instruments, beyond the boundaries of the site. No use shall be permitted which creates electrical disturbances that affect the operation of any equipment beyond the boundaries of the site.

### **4. Fire and Explosion Hazards**

All storage of and activities involving inflammable and explosive materials shall be provided with adequate safety and firefighting devices to the specifications of the City of Downey Fire marshal. All incineration is prohibited.

### **5. Odor**

No use shall be permitted which creates annoying odor in such quantities as to be readily detectable on or beyond the boundaries of the site.

## **6. Radioactivity**

The use of radioactive materials shall be limited to measuring, gauging, and calibration devices.

## **7. Vibration**

No use except a temporary construction operation shall be permitted which generates inherent and recurrent ground vibration perceptible without instruments at the boundary of the lot on which the use is located.

## **H. Existing Agreements**

This specific plan is not intended to interfere with or abrogate any easements, covenants, or other existing agreements which are more restrictive than the provisions of this specific plan.

## **I. Interpretation and Ambiguities**

1. The Community Development Director shall be responsible for the interpretation of the provisions of the Florence Avenue/I-5 specific plan. All such interpretations shall be in writing and permanently maintained. Any person aggrieved by such interpretation may request review by the Planning Commission.
2. Unless otherwise provided, any ambiguity concerning the content or application of this specific plan shall be resolved by the Community Development Director.

## **J. Nuisance**

Any use of property, building, or structure hereafter erected, built, maintained, or structurally altered contrary to the provisions of this specific plan shall be considered a public nuisance.

## **K. Severability**

In the event that any section, subsection, condition, or term of this specific plan is declared illegal or unenforceable by any court of competent jurisdiction, the other sections, subsections, conditions and terms shall remain in force and effect to the full extent permitted by law.

## **L. Violation**

It is unlawful for any person to erect, construct, enlarge, alter, repair, move, use, occupy, or maintain any building, structure, equipment, or portion thereof within the specific plan area or cause the same to be done contrary to or in violation of any provision of this specific plan.

No person shall violate any provision or fail to comply with any requirement of this specific plan. Violation of provisions herein constitutes a violation of the Downey Municipal Code.

## **M. Zone Exception Case Phase-Out**

Before 1978, the City of Downey processed fourteen applications for zone exceptions for properties within the specific plan area, and approved 11 which affected every parcel south of Florence. Specific plan 90-1 constitutes new zoning, land use, and development regulations for these properties. Upon its adoption, those zone exceptions rendered unnecessary by the new zoning and those which were granted for projects which no longer exist shall become invalid. The following is a summary of all zone exception approvals in the specific plan area.

Those on-premises signs that have been granted by zone exception, that have not lapsed or been replaced, and that do not meet the standards for signs set forth in this specific plan shall be considered legal nonconforming and shall be amortized pursuant to state laws and the Downey Municipal Code.

ZE #	SITE	YEAR	APPROVAL TO	STATUS	REASON ZE NO LONGER NECESSARY
443	7	1964	Allow Real estate office in the R1 zone	No longer exists. Now part of auto dealership, zoned C2	Now zoned for commercial uses.
516	7	1964-65	Allow auto uses, oversized signs in the R1 zone. 1' setback on Dollison	Signs no longer exist. Buildings constructed to 1' setback.	Now zoned for commercial uses. Signs described no longer existed. 1' setback included in SP90-1.
279	2	1962	Permit a service station in the R1 & C4 zones	Property is now vacant	Property subsequently zoned for service stations. Now vacant.
802	4	1968	Permit a service station and oversized signs in the R1 zone.	Auto repair business exists. Signs have been removed.	Parcel is now in proper zone for this use.
908	10, 11, 12	1969	Permit 2 service stations, 2 restaurants with special signs.	Restaurants and one station no longer exist. Now Penske Honda & Union 76. Signs for Union 76 still in place.	Station no longer exists, signs for 3 parcels as permitted by ZE no longer exist.
950	Application withdrawn				
1000	8,9	1972	Permit gas station and strip center with signs, and apartment building in R1 zone (apt not in SP 90-1 area)	Arco station, strip center, and apartments remain. Signs conform to current codes.	SP 90-1 will correct zoning inconsistency, retain restriction on 24-hr businesses. Conditions relating to apts must remain.
1016	7	1971	Expand Cadillac Dealership, 1' setback on Dollison	Superseded by ZE 1021 (Applicant didn't like conditions of 1016)	Replaced by ZE 1021, 1' setback part of SP90-1
1021	7	1971	Expand Cadillac Dealership, 1' setback on Dollison	Use still exists	Setback part of SP90-1.
1070	9	1972	Allow 50' Arco sign.	Existing sign meets current standard	Approved sign no longer exists.
1188	10, 12	1974	Allow auto sales in R1 zone	Part of Penske Honda	Now zoned correctly for auto sales
1310	Denied request for 24-hour market in mini mall				
1371	3,7	1977	Expand dealership onto Site 3, 1' setback on Dollison.	Project never completed.	Project never completed. 1' setback part of SP90-1
1409	Denied request for Arco price signs (now permitted by code)				

## ***SECTION VIII – IMPLEMENTATION/PROGRAM MONITORING***

The mitigation monitoring program developed for the EIR for Specific Plan 90-1, will be attached to the adopted version of this document. This program will outline the implementation of this specific plan and responsibilities regarding building permit issuance, design guidelines, development review, fire protection requirements, and street improvements.

## **APPENDIX A: DOWNEY MUNICIPAL CODE SECTION 9122.04**

The following section of the Downey Municipal Code lists the uses that are allowed in the C-1 (Neighborhood Commercial) Zone. These uses, except for item (ak), would be permitted on Site 8, as noted in Section III of this Specific Plan.

**SECTION 9122.04. PERMITTED USES.** The following uses shall be permitted in the C-1 Zone, and land shall be used and buildings and structures shall hereafter be erected, altered, enlarged, or otherwise modified for the following local retail business only, which are primarily intended to serve the immediate neighborhood, including the following:

- (a) Accountants;
- (b) Attorneys;
- (c) Antique sales at retail of genuine antiques;
- (d) Bakeries;
- (e) Barber and beauty shops;
- (f) Book, Stationery, and/or magazine stores (new goods only);
- (g) Cafes, coffee shops, and restaurants. The maximum seating capacity shall not exceed fifty persons;
- (h) Camera and photo supplies;
- (i) Candy stores;
- (j) Chinaware and pottery stores;
- (k) Christmas tree lots (see Section 9136.16 of this chapter);
- (l) Cleaning and laundry agencies primarily serving as collection and distribution points, laundromats, and coin-operated dry cleaning establishments;
- (m) Clothing stores, new;
- (n) Convenience markets (require conditional use permits when abutting a residential zone);
- (o) Curio shops;
- (p) Delicatessen counters serving meats, sandwiches, salads, pastries, and the like when entirely within and accessory to a grocery store or meat market;
- (q) Dentists;
- (r) Dressmaking;
- (s) Drugstores;
- (t) Electrical appliance stores;
- (t-1) Fireworks stands (see Section 9136.20 of this chapter);
- (u) Fix-it shops;
- (v) Florist shops;
- (w) Furniture stores (new merchandise only);
- (x) Furrier shops, retail;
- (y) Garden and plant nurseries and sales;
- (z) Gift and card shops;
- (aa) Grocery, fruit, and vegetable stores and fish and/or meat markets;
- (ab) Hardware stores;
- (ac) Health food stores;
- (ad) Hobby and toy stores;
- (ae) Ice cream parlors;
- (af) Ice, retail sales;
- (ag) Instruments, professional and scientific, retail only;
- (ah) Insurance offices;
- (ai) Interior decorators;
- (aj) Jewelry stores, including watch repairs;
- (ak) Liquor, on-sale, including beer and wine (see Section 9136.22 of this chapter);
- (al) Liquor stores, off-sale;
- (am) Locksmith shops;
- (an) Medical offices;
- (ao) Mimeographing, duplicating, and instant presses;
- (ap) Music stores;
- (aq) Notion, novelty, and variety stores selling at retail;
- (ar) Painting and art supplies and instruction;
- (as) Pet shops (no outdoor operations);
- (at) Photograph studios;
- (au) Radio and television sales and service;
- (av) Real estate offices;
- (aw) Record shops;
- (ay) Shoe sales and repairs, including shoe-shines;
- (az) Small appliance stores;
- (ba) Stationery stores;
- (bb) Tailor shops;
- (bc) Tobacco stores; and
- (bd) Yardage and fabric stores.